

INDEX OF SHEETS

SCALE: 1" = 2,000'

COVER SHEET ALIGNMENT & SURVEY DATA SHEET EXISTING CONDITIONS & DEMOLITION PLAN PRELIMINARY RIGHT OF WAY DATA SHEET TYPICAL SECTIONS **GENERAL NOTES EROSION AND SEDIMENT CONTROL NOTES** EROSION AND SEDIMENT CONTROL PLAN DRAINAGE AREA MAP ROADSIDE DEVELOPMENT MAINTENANCE OF TRAFFIC PLAN PAVEMENT MARKING & SIGNING PLANS PLAN SHEETS PROFILE SHEETS 3A - 4A SIDEWALK PROFILES STORM SEWER PROFILES 6(1) - 6(2) **ENTRANCE PROFILES* GEOMETRY AND GRADING PLAN*** 8(1) - 8(2) PRELIMINARY LANDSCAPE & LIGHTING NOTES & DETAILS* PRELIMINARY LANDSCAPE & LIGHTING PLAN* XS-1 - XS-7 CROSS SECTIONS

> * DENOTES SHEETS INTENTIONALLY OMITTED FROM THIS SUBMITTAL

27

CONVENTIONAL SIGNS

TOTAL NUMBER OF SHEETS

SEE SURVEY AND DESIGN LEGEND SYMBOLS ON SHEET 02A

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT.

THIS PROJECT IS TO BE CONSTRUCTED IN ACCORDANCE WITH THE DEPARTMENT'S 2016 ROAD AND BRIDGE SPECIFICATIONS, 2016 ROAD AND BRIDGE STANDARDS (INCLUDING UPDATES), 2009 MUTCD, 2011 VIRGINIA SUPPLEMENT TO THE MUTCD, 2011 VIRGINIA WORK AREA PROTECTION MANUAL, REVISION 1, AND AS AMENDED BY CONTRACT PROVISIONS AND THE COMPLETE ELECTRONIC PDF VERSION OF THE PLAN ASSEMBLY

THE ORIGINAL APPROVED TITLE SHEET(S), INCLUDING ORIGINAL SIGNATURES, ARE FILED IN THE VDOT CENTRAL OFFICE PLAN LIBRARY. ANY MISUSE OF ELECTRONIC FILES, INCLUDING SCANNED SIGNATURES, IS ILLEGAL AND ENFORCED TO THE FULL EXTENT OF THE LAW.

DESIGN PACKAGE (AutoCAD CIVIL 3D) Computer Identification No. (UPC 113124)



FHWA-534 DATA 27001

ADT (2018)*

ADT (2038)*

D (%) (design hour)*

T (%) (design hour)*

25 mph

U000-151-R18, SEE TABULATION P101, R201, C501 BELOW FOR SECTION NUMBERS FOR SECTION NUMBERS)

PLANS SUBJECT

TO CHANGE

FOR

INFORMATION ONLY

NOT FOR

CONSTRUCTION

GS-8 URBAN LOCAL STREET - 25 MPH MINIMUM DESIGN SPEED

To: WESTERN TERMINUS OF ORCHARD ST

Fr: INTERSECTION OF FARR AVE. AND U.S. RTE. 29 (FAIRFAX BLVD.)

COMMONWEALTH OF VIRGINIA FUNCTIONAL CLASSIFICATION AND TRAFFIC DATA

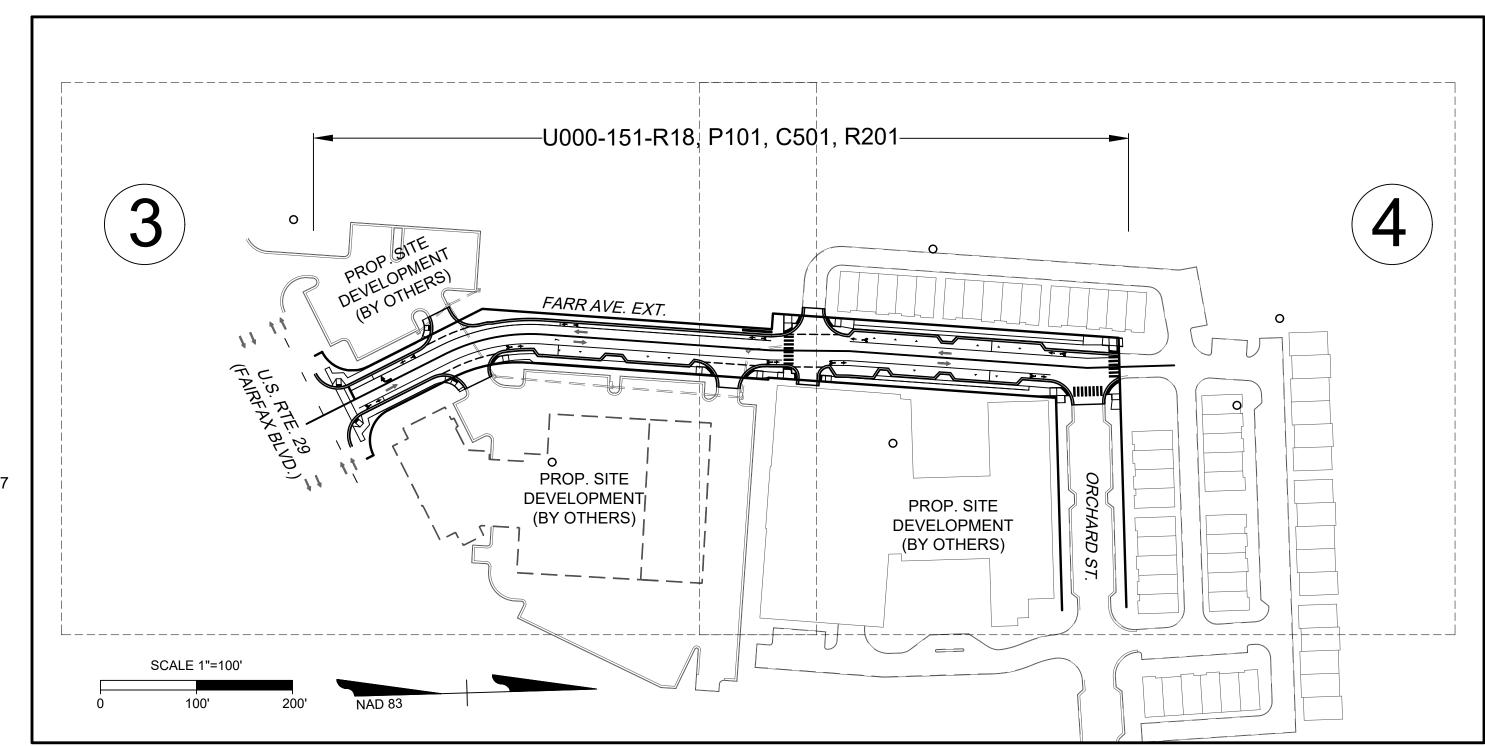
PLAN AND PROFILE OF PROPOSED STATE HIGHWAY

CITY OF FAIRFAX, VIRGINIA

FARR AVENUE EXTENSION (NORTHFAX WEST)

FROM: INTERSECTION OF FARR AVE. AND U.S. RTE. 29 (FAIRFAX BLVD.) TO: WESTERN TERMINUS OF ORCHARD ST. (38°51'27" N, 77°18'37" W)

PUBLIC HEARING PLANS



DESCRIPTION REFERENCE INTERSECTION OF FARR AVE. AND FAIRFAX BLVD. (RTE. 29) POI STA. 10+00.00

04/28/2020

Population 24,097 (U.S. Census Bureau - 2017)

STATE PROJECT	SECTION	FEDERAL AID PROJECT NO.				TYPE CODE	UPC NO.	EQUALITIES	LENGTH INCLUDING BRIDGE(S)		LENGTH EXCLUDING BRIDGE(S)		BRIDGE PROJECT	TYPE PROJECT	DESCRIPTION	
NO.			OODL	NO.	INO.	INO.		110.	140.	110.	FEET	FEET FEET MILES	MILES	FEET	MILES	NO.
	P101		PENG	113124	NONE	874.27	0.166	874.27	0.166		PRELIMINARY	Fr: INTERSECTION OF FARR AVE. AND U.S. RTE. 29 (FAIRFAX BLVD.)				
318												To: WESTERN TERMINUS OF ORCHARD ST.				
51-F	R201		ROW	113124	NONE	874.27	0.166	874.27	0.166		RIGHT OF WAY	Fr: INTERSECTION OF FARR AVE. AND U.S. RTE. 29 (FAIRFAX BLVD.)				
-14												To: WESTERN TERMINUS OF ORCHARD ST.				
000	C501		CONSTR.	113124	NONE	824.27	0.166	874.27	0.166		CONSTR.	Fr: INTERSECTION OF FARR AVE. AND U.S. RTE. 29 (FAIRFAX BLVD.)				
	>											To: WESTERN TERMINUS OF ORCHARD ST.				
Ī																

Project Lengths are based on Construction Baseline.

	TIER 1 PROJECT								
LO	LOCALLY ADMINISTERED PROJECTS								
	NAME OF LOCALITY								
	(SIGNATURE)								
NAME OF	RESPONSIBLE LOCAL GOVERMENT OFFICAL (TYPED)								
RECOMMEN	RECOMMENDED FOR APPROVAL FOR RIGHT OF WAY ACQUISITION								
DATE	TITLE OF POSITION								
	(SIGNATURE)								
NAME OF	RESPONSIBLE LOCAL GOVERMENT OFFICAL (TYPED)								
RECC	MMENDED FOR APPROVAL FOR CONSTRUCTION								
DATE	TITLE OF POSITION								
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PROPOSED ROADWAY CONSTRUCTION ALIGNMENT DATA

Station Range: Start: 500+00.00, End: 500+67.98

Station Range: Start: 600+00.00, End: 600+79.88

N 6,998,277.4816 E 11,821,692.2302

N 6,998,277.4816 E 11,821,772.1131

N 6,998,277.4816 E 11,821,772.1131

500+00.00

500+67.98

500+67.98

600+00.00

600+79.88

600+79.88

Alignment Name: Entr. #5

S84° 02' 10.14"E 67.980'

Alignment Length: 67.980'

Alignment Name: Orchard St.

N90° 00' 00.00"E 79.883'

Alignment Length: 79.883'

Begin Orchard St.

Line (1)

Line (1)

End Orchard St.

N 6,997,989.5890 E 11,821,666.0024

N 6,997,982.5259 E 11,821,733.6139

N 6,997,982.5259 E 11,821,733.6139

Begin Entr. #5

Line (1)

Line (1)

End Entr. #5

		CONSTRUCTION ALIGNMEN	NIDATA
Alignment Name: Fairfax Blvd. Station Range: Start: 20+00.00, End: 23+18.54		**************************************	
Begin Fairfax Blvd. N 6,997,413.2460 E 11,821,573.5800	20+00.00	Begin Entr. #1 N 6,997,621.9380 E 11,821,653.0253	100+00.00
Line (1) N64° 57' 52.57"E 318.544' N 6,997,548.0470 E 11,821,862.1960 Line (1)	23+18.54	Line (1) S64° 53' 30.50"W 60.239' N 6,997,596.3770 E 11,821,598.4788 Line (1)	100+60.24
N 6,997,548.0470 E 11,821,862.1960 End Fairfax Blvd.	23+18.54	N 6,997,596.3770 E 11,821,598.4788 End Entr. #1	100+60.24
**************************************		**************************************	
Alignment Name: Farr Ave Ext Station Range: Start: 10+00.00, End: 19+25.56		Alignment Name: Entr. #2 Station Range: Start: 200+00.00, End: 200+6	
************	******	*******************	*******
Begin Farr Ave Ext N 6,997,463.0385 E 11,821,726.8751	10+00.00	Begin Entr. #2 N 6,997,626.3257 E 11,821,651.4133	200+00.00
Line (1) N25° 06' 29.50"W 160.345'		Line (1) N65° 51' 20.52"E 63.174'	
N 6,997,608.2326 E 11,821,658.8359 Line (1)	11+60.35	N 6,997,652.1660 E 11,821,709.0604 Line (1)	200+63.17
Curve (2) BC N 6,997,608.2326 E 11,821,658.8359 CTR N 6,997,693.0984 E 11,821,839.9376 PI N 6,997,658.1272 E 11,821,635.4549	11+60.35	N 6,997,652.1660 E 11,821,709.0604 End Entr. #2	200+63.17
11 14 0,007,000.1212 2 11,021,000.1010		Alignment Length: 63.174'	
Direction Back N25° 06' 29.50"W Radius 200.000'		****************	*******
Delta 30°48'23"(RT) Length 107.535'		Alignment Name: Entr. #3	
Tangent 55.101' Chord Direction N9° 42' 18.00"W Distance Direction Ahead N5° 41' 53.50"E	e 106.244'	Station Range: Start: 300+00.00, End: 300+62	2.35 *********
Direction Ariead No. 41 55.50 E		Begin Entr. #3	
EC N 6,997,712.9561 E 11,821,640.9258 Curve (2)	12+67.88	N 6,997,907.5138 E 11,821,660.3391	300+00.00
Line (2)		Line (1) S84° 18' 06.50"E 62.353'	
Line (3) N5° 41' 53.50"E 236.339' N 6,997,948.1269 E 11,821,664.3915	15+04.22	N 6,997,901.3229 E 11,821,722.3840 Line (1)	300+62.35
Line (3)	-	N 6,997,901.3229 E 11,821,722.3840	300+62.35
Line (4) **Non-Tangent** Radial Bearing N84° 18' 06.5	0"W	End Entr. #3	
N2° 13' 29.62"E 82.531' N 6,998,030.5955 E 11,821,667.5955	15+86.75	Alignment Length: 62.353'	
Line (4)		************	
Line (5) N5° 41' 53.50"E 248.112'		Alignment Name: Entr. #4	
N 6,998,277.4816 E 11,821,692.2302 Line (5)	18+34.86	Station Range: Start: 400+00.00, End: 400+80	
Line (6) **Non-Tangent** Radial Bearing N84° 18' 06.5 N0° 00' 00.00"E 90.703'	0"W	Begin Entr. #4 N 6,997,990.1289 E 11,821,666.0233	400+00.00
N 6,998,368.1848 E 11,821,692.2302 Line (6)	19+25.56	Line (1) N84° 18' 06.50"W 80.381'	
N 6,998,368.1848 E 11,821,692.2302 End Farr Ave Ext	19+25.56	N 6,997,998.1098 E 11,821,586.0397 Line (1)	400+80.38
**************************************	******	N 6,997,998.1098 E 11,821,586.0397	400+80.38

End Entr. #4

Alignment Length: 80.381'

Alignment Length: 925.565'

SURVEY CONTROL DATA

BENCHMARK DATA TO BE PROVIDED FOR CONSTRUCTION STAKEOUT. ADDITIONAL INFORMATION TO BE PROVIDED WITH SUBSEQUENT SUBMITTALS.

PLANS SUBJECT TO CHANGE FOR INFORMATION ONLY NOT FOR CONSTRUCTION

04/28/2020 DRAWN BY DESIGNED BY CHECKED BY NJS SCALE 1"=25'

41013.009



PRELIMINARY RIGHT-OF-WAY DATA TABLE

				AREA									
	· ·		SHEET NO.	TOTAL	FEE TAKING	FEE REMAINDER	EASEMENTS						
PARCEL	LANDOWNER	PARCEL ID					UTILITY		PERMANENT		TEMPORARY		
NO.	LANDOWNER						PUBLIC	VERIZON	DRAINAGE	SLOPE	STORMWATER MANAGEMENT	CONSTRUCTION	
				ACRES	ACRES	ACRES	ACRES	ACRES	ACRES	ACRES	ACRES	ACRES	YES/NO
001	N/F ALPINE SCHUILING T. INC.	57-2-02-010	03	N/A	0.572	N/A	-	-	-	-	-	-	NO
002	N/F ALPINE SCHUILING T. INC.	57-2-02-009	03	N/A	0.009	N/A	-	-	-	-	-	-	NO
003	NAPOLITANO GROUP PARCELS	57-2-02-003 57-2-02-005 57-2-08-008 57-2-08-010	04	RIGHT OF WAY VACATION & DEDICATION BY OTHERS - REFER TO PLAN BY CCL.									

NOTE: PRELIMINARY RIGHT-OF-WAY DATA TABLE CURRENTLY REFLECTS ONLY FEE R/W ACQUISITIONS. PERMANENT AND TEMPORARY EASEMENTS ARE EXPECTED TO BE REQUIRED FOR CONSTRUCTION AND ONGOING MAINTENANCE OF SITE FEATURES AND WILL BE SHOWN ON THE PLANS AND REFLECTED IN THIS TABLE IN SUBSEQUENT SUBMITTALS.

NOTE: THIS PRELIMINARY RIGHT-OF-WAY DATA TABLE ASSUMES THAT THE PLOT OF LAND MARKED BY THE DEED OVERLAP BETWEEN BRANCH AVENUE ASSOCIATES, LLC AND ALPINE SCHUILING T. INC. IS OWNED BY ALPINE SCHUILING T. INC. COORDINATION REGARDING THIS DEED OVERLAP IS ONGOING AND PLANS AND FEE R/W AREAS WILL BE UPDATED AS NECESSARY.

PLANS SUBJECT TO CHANGE

FOR INFORMATION ONLY

NOT FOR CONSTRUCTION

04/28/2020 DRAWN BY DESIGNED BY NJS

CHECKED BY SCALE

A AVENUE EXT. (NORTHFAX VICTIVIA)

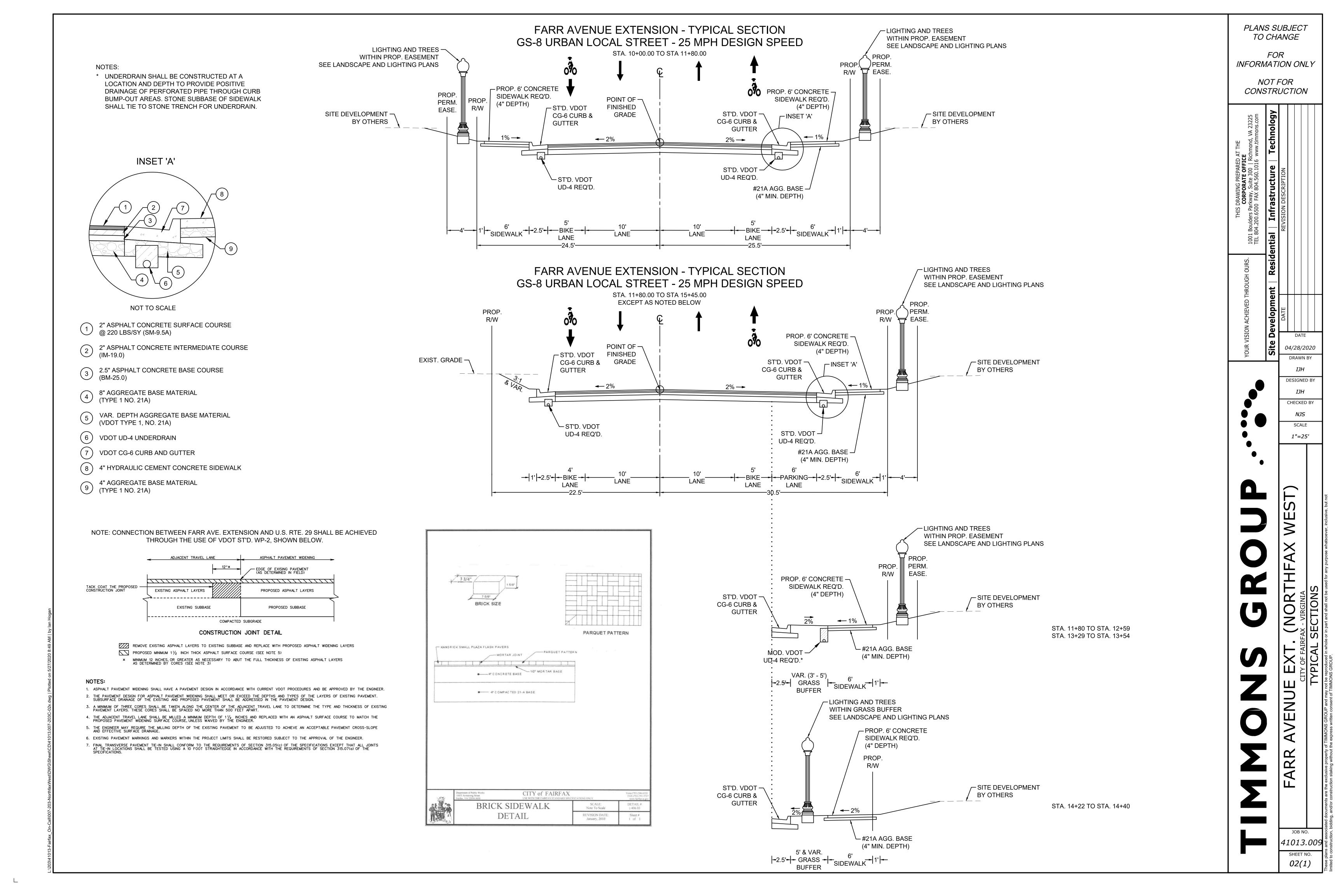
CITY OF FAIRFAX - VIRGINIA

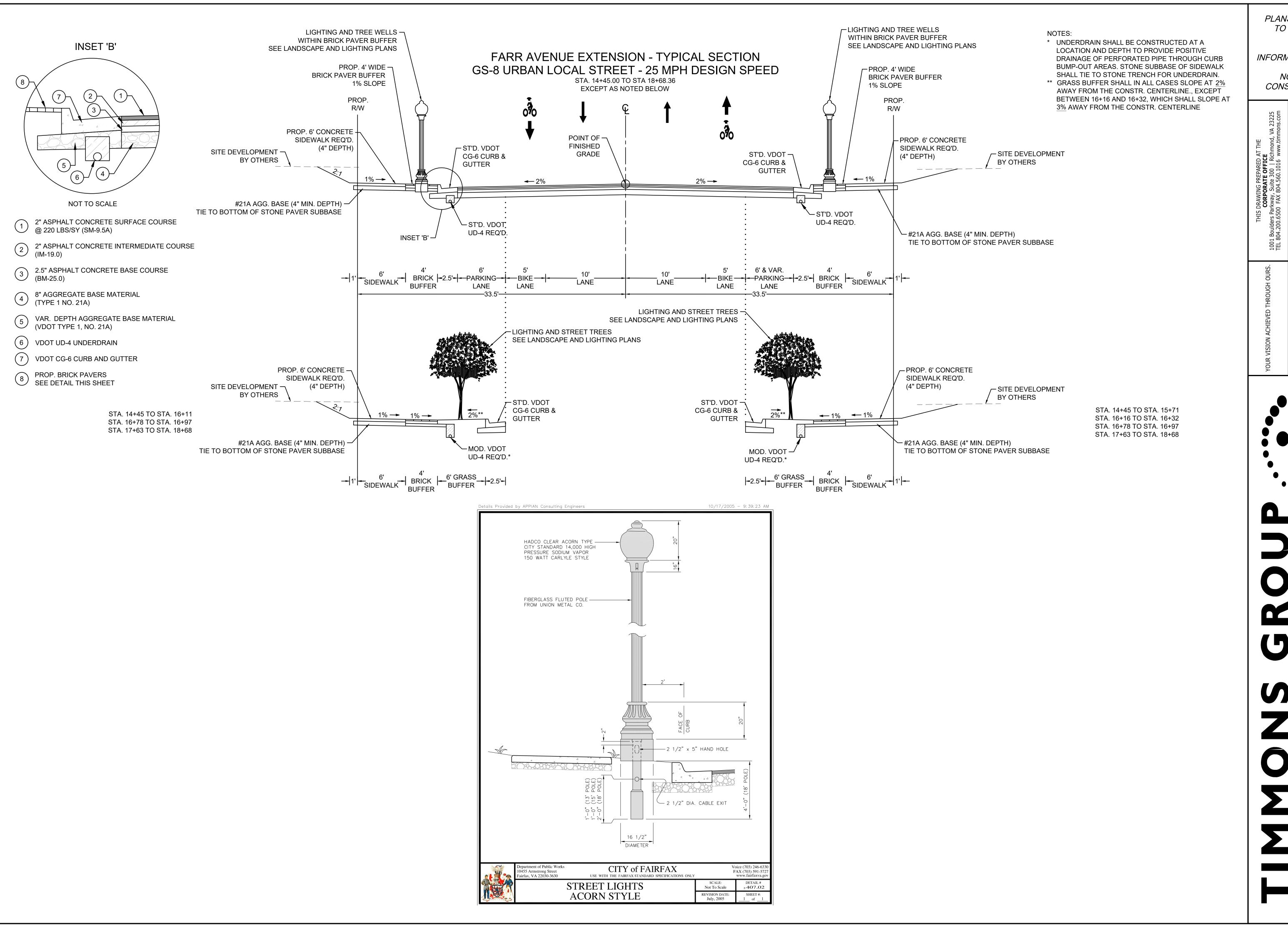
PRELIMINARY RIGHT OF WAY DATA SHEET

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JOB NO. 41013.009

01C





INFORMATION ONLY

NOT FOR CONSTRUCTION

04/28/2020 DRAWN BY

> DESIGNED BY CHECKED BY NJS SCALE

1"=25'

JOB NO.

41013.009 SHEET NO. 02(2)

CITY OF FAIRFAX GENERAL CONSTRUCTION NOTES

PERMITS

- 1. A STREET OPENING PERMIT IS REQUIRED FOR ANY WORK IN A CITY RIGHT-OF-WAY OR EASEMENT. THE PERMIT CAN BE OBTAINED FROM THE PUBLIC WORKS DEPARTMENT. FOR INFORMATION, CALL 703-385-7980 OR 703- 385-7810.
- 2. ALL SIDEWALKS, CURBS, GUTTERS, DRIVEWAYS, STREETS, STORM PIPES, SANITARY SEWER, ENDWALLS AND RIP- RAPS MUST BE INSPECTED BY THE CITY. ALL WORK IN THE CITY STREETS WILL BE PERFORMED MONDAY- FRIDAY BETWEEN THE HOURS OF 9:00 A.M. AND 3:00 P.M. NO WORK IS TO BE PERFORMED ON WEEKENDS OR HOLIDAYS UNLESS PRE-APPROVED BY THE DIRECTOR OF PUBLIC WORKS.
- 3. INSPECTIONS PERFORMED BY THE FACILITIES INSPECTOR WILL REQUIRE A D-1 FOUR-HOUR NOTICE PRIOR TO INSPECTIONS

GENERAL STANDARDS

THE PUBLIC WORKS DIRECTOR MUST BE NOTIFIED ONE WEEK PRIOR TO PRE-CONSTRUCTION CONFERENCE, ONE WEEK PRIOR TO COMMENCEMENT OF LAND DISTURBING ACTIVITY AND ONE WEEK PRIOR TO FINAL INSPECTION. THE SITE PLAN COORDINATOR IN ZONING (703-385-7820) MUST BE NOTIFIED ONE WEEK PRIOR TO THE PRE-CONSTRUCTION CONFERENCE.

- 4. A PRECONSTRUCTION MEETING WILL BE REQUIRED THREE DAYS PRIOR TO ANY CONSTRUCTION. CONTRACTORS WILL NOTIFY THE PUBLIC WORKS DEPARTMENT OR FACILITIES INSPECTOR FOR ALL WORK DONE ON SITE AND OFF SITE ONE DAY PRIOR TO STARTING.
- 5. THE CONTRACTOR SHALL PROVIDE ADEQUATE MEANS FOR PARKING CONSTRUCTION EQUIPMENT AND PROVIDE EMPLOYEE PARKING ON SITE.
- 6. ALL CONSTRUCTION SHALL CONFORM TO THE LATEST CITY OF FAIRFAX STANDARDS, VIRGINIA DEPARTMENT OF TRANSPORTATION AND THE VIRGINIA SEDIMENT & EROSION CONTROL CURRENT SPECIFICATIONS, EXCEPT AS SHOWN OR ALTERED BY THESE PLANS.
- 7. TRAFFIC SIGNS FOUND TO BE IN THE WAY AT CONSTRUCTION SITES SHALL BE REMOVED OR RELOCATED ONLY BY PERSONNEL IN THE SIGN & SIGNAL CREW OF THE PUBLIC WORKS DEPARTMENT AT THE CONTRACTOR'S REQUEST. ANY CONTRACTOR FOUND RESPONSIBLE FOR MOVING CITY PROPERTY WITHOUT PERMISSION WILL RECEIVE A SUMMONS.
- 8. AN AS-BUILT PLAN MUST BE SUBMITTED WITHIN 30 DAYS AFTER COMPLETION OF ALL CONSTRUCTION.
- 9. TEMPORARY STRUCTURES, CONSTRUCTION TRAILERS AND DEMOLITION REQUIRE PERMITS FROM THE OFFICE OF CODE ADMINISTRATION PRIOR TO START OF WORK OR INSTALLATION.
- 10. CITY ORDINANCE PERMITS CONSTRUCTION NOISE, INCLUDING EXCAVATION, BETWEEN THE HOURS OF 7:00 AM AND 6:00 PM ON WEEKDAYS AND 8:30 AM AND 5:00 PM ON SATURDAYS ONLY. IT SHALL BE THE RESPONSIBILITY OF THE DEVELOPER TO ENSURE THAT ALL CONTRACTORS AND SUBCONTRACTORS COMPLY WITH THIS ORDINANCE.
- 11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING COMPLIANCE WITH CITY CODE SECTIONS LIMITING GROWTH OF GRASS AND WEEDS TO SIX INCHES IN HEIGHT.

ENVIRONMENTAL

- 1. ALL EROSION SILTATION CONTROL TO BE INSTALLED PRIOR TO STARTING PROJECT TO CONFORM TO THE CURRENT VIRGINIA EROSION AND SEDIMENT CONTROL MANUAL
- 2. THE CONTRACTOR SHALL PROVIDE ADEQUATE MEANS OF CLEANING MUD FROM TRUCKS AND/OR OTHER EQUIPMENT PRIOR TO ENTERING THE CITY OF FAIRFAX RIGHTS-OF-WAY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO CLEAN STREETS AND ALLAY DUST AND TO TAKE WHATEVER MEASURES NECESSARY TO ENSURE THAT THE ROAD IS MAINTAINED IN A CLEAN AND DUST-FREE CONDITION AT ALL TIMES.
- 3. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PERFORM THE WORK IN SUCH A ADJACENT PROPERTIES.
- 4. IF THE PRESENCE OF ASBESTOS IS SUSPECTED IN THE SOIL, THE CONTRACTOR MUST CONTACT THE AIR POLLUTION CONTROL DIVISION OF THE FAIRFAX COUNTY HEALTH DEPARTMENT AT 703-246-2300.
- 5. ONSITE STORAGE OF FUEL SHALL BE LIMITED TO DIESEL FUEL TANKS NOT OVER 660 GALLONS CAPACITY. TANKS SHALL BE OF A LISTED TYPE AND SHALL BE PROVIDED WITH APPROVED SECONDARY CONTAINMENT, IMPACT PROTECTION AND PLACARDING. A MINIMUM 2A-40BC FIRE EXTINGUISHER SHALL BE PROVIDED IN THE VICINITY OF THE REFUELING AREA. A PERMIT FOR COMBUSTIBLE LIQUID STORAGE SHALL BE OBTAINED FROM THE OFFICE OF CODE ADMINISTRATION, 703-385-7830. FUEL SHALL NOT BE PLACED IN ONSITE STORAGE TANKS UNTIL THE INSTALLATION HAS BEEN INSPECTED AND APPROVED.
- 6. ONSITE REPAIR OF VEHICLES AND EQUIPMENT SHALL BE LIMITED TO REPLACEMENT OF DAMAGED BELTS, HOSES AND TIRES. ANY SPILL OF FUEL, OIL, HYDRAULIC FLUID OR ANTI-FREEZE GREATER THAN ONE GALLON MUST BE REPORTED TO THE OFFICE OF CODE ADMINISTRATION AT 703-385-7830. ALL SPILLS MUST BE CLEANED UP PROMPTLY AND IN AN APPROVED MANNER.
- 7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING COMPLIANCE WITH CITY CODE SECTIONS REGARDING HEALTH AND SAFETY MENACES, INCLUDING ACCUMULATIONS OF WATER, STORAGE OF MATERIAL, CONSTRUCTION DEBRIS AND SECURITY OF THE SITE.
- 8. THE LINK TO THE ASBESTOS INFORMATION AND MAP ON THE FAIRFAX COUNTY WEBSITE HTTP://WWW.FAIRFAXCOUNTY.GOV/HD/ASB/

LANDSCAPE

- 1. THE AREA SURROUNDING ALL TREES, SHRUBS AND GROUNDCOVER SHALL BE TOPPED WITH TWO INCHES OF SHREDDED HARDWOOD BARK MULCH.
- 2. NO CHANGES SHALL BE PERMITTED TO THE PLANT LIST UNLESS APPROVED BY THE CITY OF FAIRFAX.
- 3. TREES SHALL BE CLASSIFIED AS PER "AMERICAN STANDARD FOR NURSERY STOCK" AS ADOPTED BY THE AMERICAN ASSOCIATION OF NURSERYMEN. PLANT MATERIAL BELOW THIS STANDARD SHALL NOT BE CONSIDERED.
 - a) ALL PLANTS MUST CONFORM TO REQUIREMENTS PER PLANT LIST;
- b) ALL PLANT MATERIALS MUST BE NURSERY GROWN STOCK;
- c) ALL TREES MUST BE WELL BRANCHED, FULL CROWN.

VDOT GENERAL NOTES

GRADING

- G-1 THE GRADE LINE DENOTES TOP OF FINISHED PAVEMENT UNLESS SHOWN OTHERWISE ON TYPICAL SECTIONS OR PLANS.
- G-4 THE COST OF REMOVAL OF ALL EXISTING CONCRETE ITEMS LOCATED IN THE AREA TO BE GRADED, INCLUDING, BUT NOT LIMITED TO THE FOLLOWING, SHALL BE INCLUDED IN THE PRICE BID FOR REGULAR EXCAVATION: CURB, SIDEWALK, FOUNDATIONS, DRAINAGE STRUCTURES, ETC.
- THE BORROW MATERIAL FOR THIS PROJECT SHALL BE A MINIMUM CBR 10 OR AS APPROVED BY THE MATERIALS ENGINEER.

DRAINAGE

- THE HORIZONTAL LOCATION OF ALL DRAINAGE STRUCTURES SHOWN ON THESE PLANS IS APPROXIMATE ONLY. WITH THE EXCEPTION OF STRUCTURES SHOWING SPECIFIC STATIONS, SPECIAL DESIGN BRIDGES AND STORM SEWER SYSTEMS
- D-2 THE HORIZONTAL LOCATION AND INVERT ELEVATIONS SHOWN FOR PROPOSED CULVERTS AND STORM SEWER OUTFALL PIPES ARE BASED ON EXISTING SURVEY DATA AND REQUIRED DESIGN CRITERIA. IF DURING CONSTRUCTION, IT IS FOUND THAT THE HORIZONTAL LOCATION OR INVERT ELEVATIONS SHOWN ON THE PLANS DIFFER SIGNIFICANTLY FROM THE HORIZONTAL LOCATION OR ELEVATIONS OF THE STREAM OR SWALE IN WHICH THE CULVERT OR STORM SEWER OUTFALL PIPE IS TO BE PLACED, THE ENGINEER SHALL CONFER WITH, AND GET APPROVAL FROM, THE APPLICABLE DISTRICT DRAINAGE ENGINEER BEFORE INSTALLING THE CULVERT OR STORM SEWER OUTFALL
- THE "H" DIMENSIONS SHOWN ON PLANS FOR DROP INLETS AND JUNCTION BOXES AND THE "L.F. " DIMENSIONS SHOWN FOR MANHOLES ARE FOR ESTIMATING PURPOSES AND ARE BASED ON THE PROPOSED INVERT ELEVATIONS SHOWN FOR THE STRUCTURE AND THE ANTICIPATED TOP (RIM) ELEVATION BASED ON EXISTING OR PROPOSED FINISHED GRADE. THE ACTUAL "H" OR "LF. " DIMENSIONS ARE TO BE DETERMINED BY THE CONTRACTOR FROM FIELD CONDITIONS.
- D-7 ALL PIPE ON THIS PROJECT SHALL BE REINFORCED CONCRETE PIPE, CLASS III (MIN.) FOR STRENGTH, SHEET THICKNESS, OR CLASS DESIGNATION; AVAILABLE SIZES; HEIGHT OF COVER LIMITATIONS; AND OTHER RESTRICTIONS FOR A PARTICULAR PIPE TYPE OR HEIGHT COVER, SEE THE APPLICABLE SECTIONS OF THE VDOT ROAD AND BRIDGE STANDARDS PC-1.
- D-8 WHERE OPEN JOINT PIPE IS TO BE USED, NO JOINT SHALL BE OPENED A DISTANCE EXCEEDING 25% OF THE SPIGOT LENGTH. SEALING OF THE PIPE JOINT SHALL BE IN ACCORDANCE WITH SECTION 302 OF THE APPLICABLE VDOT ROAD AND BRIDGE SPECIFICATIONS.
- D-12 ALL EXISTING DRAINAGE FACILITIES LABELED "TO BE ABANDONED" SHALL BE LEFT IN PLACE, BACKFILLED AND PLUGGED IN ACCORDANCE WITH THE VDOT ROAD AND BRIDGE STANDARD PP-1. BASIS OF PAYMENT WILL BE C.Y. OF FLOWABLE BACKFILL.
- D-13 EXISTING DRAINAGE FACILITIES BEING UTILIZED AS A PART OF THE DRAINAGE SYSTEM, AND DESIGNATED ON THE PLANS "TO BE CLEANED OUT" SHALL BE CLEANED AS DIRECTED BY THE ENGINEER. THE COST INCIDENTAL TO THIS SHALL BE INCLUDED IN THE CONTRACT PRICE FOR OTHER ITEMS.
- D-14 PROPOSED DROP INLETS WITH A HEIGHT (H) LESS THAN THE STANDARD MINIMUM SHOWN IN THE VDOT ROAD AND BRIDGE STANDARDS SHALL BE CONSIDERED AND PAID FOR AS STANDARD DROP INLETS FOR THE TYPE SPECIFIED.
- D-16 WHEN CG-6 OR CG-7 IS SPECIFIED ON A RADIUS (SUCH AS AT A STREET INTER-SECTION) THE ENGINEER MAY APPROVE A DECREASE IN THE CROSS SLOPE OF THE GUTTER TO FACILITATE PROPER DRAINAGE.

PAVEMENT

THE PAVEMENT MATERIALS ON THIS PROJECT WILL BE PAID FOR ON A TONNAGE BASIS THE WEIGHT WILL VARY IN ACCORDANCE WITH THE SPECIFIC GRAVITY OF THE AGGREGATES AND THE ASPHALTIC CONTENT OF THE MIX ACTUALLY USED TO SECURE THE DESIGN DEPTH. THE WEIGHT OF THE ASPHALT CONCRETE IS BASED ON 95% OF THE THEORETICAL MAXIMUM DENSITY.

INCIDENTALS

- THAT PORTION OF THE RIGHT OF WAY LYING WITHIN THE CLEAR ZONE OR WITHIN A MINIMUM OF 10 FEET FROM THE EDGE OF PAVEMENT OR SURFACING OR WITHIN THE LIMITS OF THE CONSTRUCTION SLOPES BEYOND 10 FEET, SHALL BE CLEARED AND GRUBBED IN ACCORDANCE WITH THE APPLICABLE VDOT ROAD AND BRIDGE SPECIFICATIONS, SECTION 301. WHERE SUFFICIENT RIGHT OF WAY OR CONSTRUCTION EASEMENT IS PROVIDED.
- I-6 CERTAIN TREES SHALL BE PRESERVED AS NOTED ON PLANS OR AS DIRECTED BY THE
- MANNER TO PREVENT THE WASHING OF ANY TOPSOIL, SILT, OR DEBRIS ONTO I-8A CLEARING AND GRUBBING SHALL BE CONFINED TO THOSE AREAS NEEDED FOR CON-STRUCTION. NO TREES OR SHRUBS IN UNGRADED AREAS SHALL BE CUT WITHOUT THE PERMISSION OF THE ENGINEER.
 - WHEN NO CENTERLINE ALIGNMENT IS SHOWN FOR A PROPOSED ENTRANCE. THE ENTRANCE SHALL BE CONSTRUCTED IN THE SAME LOCATION AS THE EXISTING ENTRANCE.
 - I-12 ST'D. RM-2 RIGHT OF WAY MONUMENTS SHALL BE SET BY THE CONTRACTOR.

AND GUTTER SECTIONS, SEE TYPICAL DETAILS ON SHEET 2A.

- I-16 THE "UNDERGROUND UTILITIES" SURVEY DATA ON THIS PROJECT HAS BEEN PROVIDED BY CONSULTANT AND COPIES ARE AVAILABLE FROM THE DEPARTMENT.
- I-17 FOR METHOD OF CONSTRUCTING STRAIGHT-LINE TAPER LANES IN CURB AND/OR CURB
- I-18 ALL PAVEMENT MARKINGS AND TRAFFIC FLOW ARROWS SHOWN ON THE ROADWAY CON-STRUCTION PLANS ARE SCHEMATIC ONLY. THE ACTUAL LOCATION AND APPLICATION OF PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH SECTION 704 OF THE APPLICABLE VDOT ROAD AND BRIDGE SPECIFICATIONS, MUTCD, SEQUENCE OF CONSTRUCTION/ TRAFFIC CONTROL PLANS, PAVEMENT MARKING PLAN SHEETS, AND AS DIRECTED BY THE ENGINEER.
- I-19 THE FOLLOWING SOURCES, UNDER CONTRACT WITH VDOT, HAVE PROVIDED INFORMATION ON THIS PROJECT:

HYDRAULIC DESIGN - TIMMONS GROUP ROADWAY DESIGN - TIMMONS GROUP UTILITY DESIGN - TIMMONS GROUP UTILITY DESIGNATION - TIMMONS GROUP UTILITY LOCATION - TIMMONS GROUP SURVEY - TIMMONS GROUP

BRIDGE DESIGN - N/A

IF QUESTIONS OR PROBLEMS ARISE DURING CONSTRUCTION, PLEASE CONTACT THE PROJECT DESIGNER. DO NOT CONTACT THE OUTSIDE SOURCES.

1-20 THE OFFICIAL ELECTRONIC PDF VERSION OF THE PLANS WILL OVERRIDE THE PAPER COPIES OR PRINTS OF SPECIFIC LAYERS.

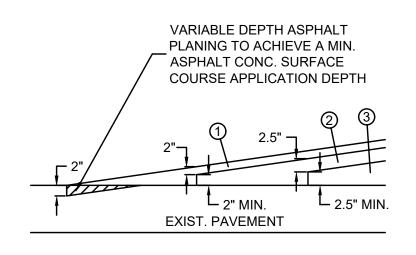
PORTIONS OF THIS PLAN ASSEMBLY HAVE BEEN CADD GENERATED. TO ASSIST IN THE PREPARATION OF THE BID AND CONSTRUCTION OF THE PROJECT, AUTOCAD FORMAT (.DWG) FILES WILL BE MADE AVAILABLE TO THE PRIME CONTRACTOR DURING BIDS AND AFTER AWARD OF THE CONTRACT.

ALL ELECTRONIC PLAN ASSEMBLIES WILL INCLUDE THE CONSTRUCTION PLANS IN TWO FORMATS: PDF FILES AND AUTOCAD FORMAT (.DWG) FILES. ONLY THE PDF FILES WILL BE CONSIDERED AS PART OF THE OFFICIAL PLAN ASSEMBLY.

THE AUTOCAD FORMAT (.DWG) FILES ARE FURNISHED ONLY AS INFORMATION FOR THE CONTRACTOR. THESE PLANS ARE DEVELOPED IN LAYERS TO AID IN READABILITY HOWEVER, THE CONSTRUCTION ITEMS MAY OR MAY NOT BE IN THE PROPER LAYERING SCHEME AS DESCRIBED IN THE VDOT CADD MANUAL. THE AUTOCAD FILES WILL ONLY MATCH THE SCANNED FILES IF ALL REQUIRED LEVELS ARE TURNED ON. A AUTOCAD SOFTWARE LICENSE IS REQUIRED TO BE ABLE TO READ THESE FILES.

EROSION AND SEDIMENT CONTROL (ESC)

- IF THE REMOVAL OF BRUSH SILT BARRIER IS SPECIFIED BY THE PLANS OR REQUIRED BY THE ENGINEER, THE COST OF REMOVAL AND DISPOSAL OF BRUSH SHALL BE IN ACCORDANCE WITH SECTION 109 OF THE APPLICABLE VDOT ROAD AND BRIDGE SPECIFICATIONS.
- ROCK FOR CHECK DAMS, INLET PROTECTION, EROSION CONTROL STONE AND RIPRAP SHALL BE IN ACCORDANCE WITH SECTION 203 AND SECTION 414 OF THE APPLICABLE VDOT ROAD AND BRIDGE SPECIFICATIONS.
- REFER TO THE EROSION AND SEDIMENT CONTROL PLANS FOR A LEGEND OF SYMBOLS DEPICTING THOSE ITEMS.

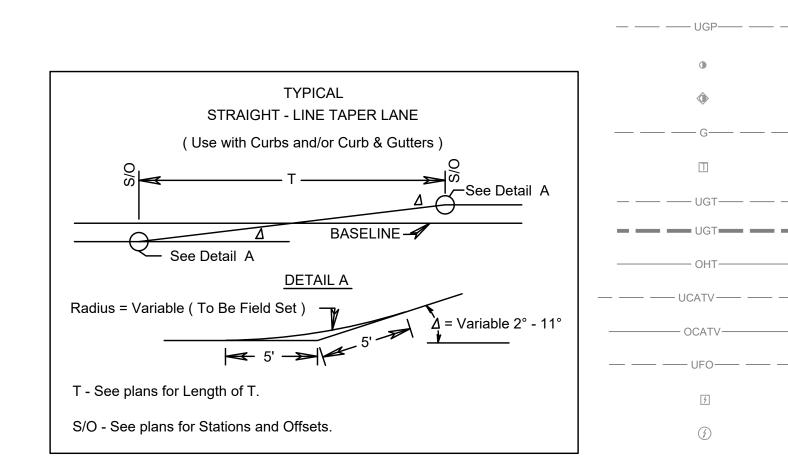


PAVEMENT BUILD-UP WITH OVERLAY

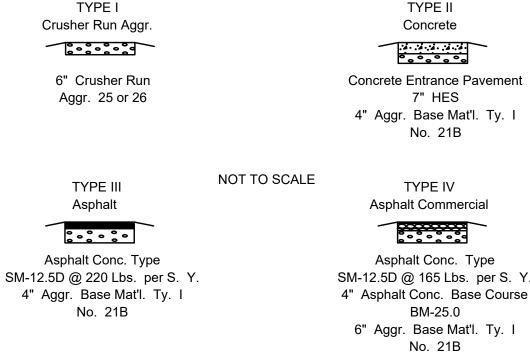
- ASPHALT CONC. OVERLAY TY. SM-12.5D (2" MIN. DEPTH REQ'D.)
- VAR. DEPTH IM-19.0A ASPHALT LEVELING COURSE (DEPTHS UP TO 2.5")

ASPHALT TACK COURSE

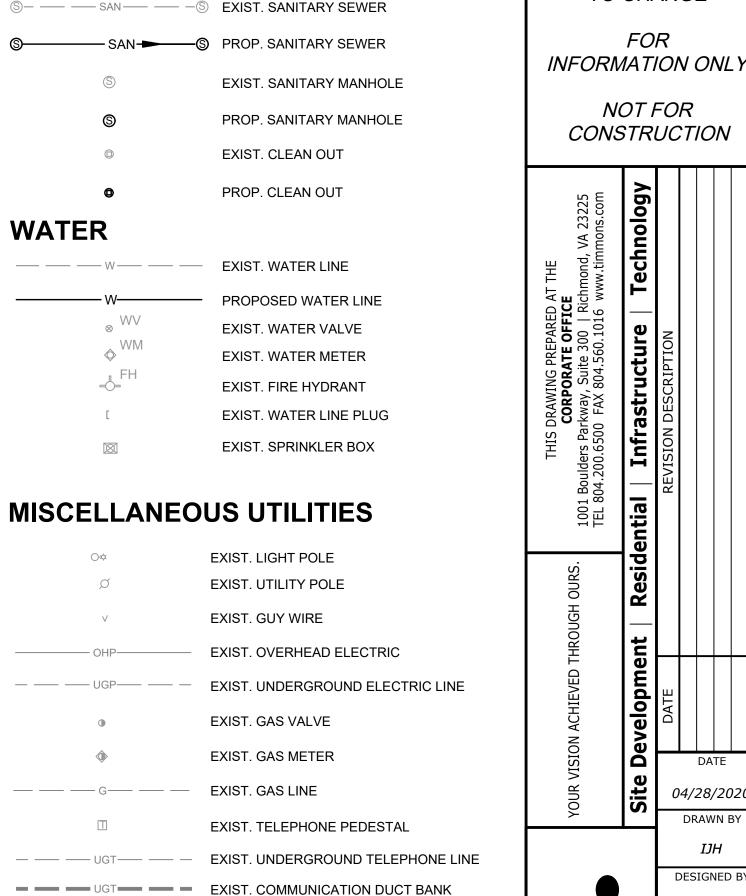
- VAR. DEPTH BM-25.0A ASPHALT LEVELING COURSE (REMAINDER IF NECESSARY)
- CONTRACTOR MAY USE FULL DEPTH SM-12.5A ASPHALT OVERLAY IF SO DESIRED.



PRIVATE AND COMMERCIAL ENTRANCES



The type of entrance (I, II, III, IV) to be constructed will be determined by the existing condition at the time of construction.



SEWER

NOT FOR CONSTRUCTION ING PREPAR
ORATE OFF
, Suite 300

PLANS SUBJECT TO CHANGE

04/28/2020 DRAWN BY

DESIGNED BY CHECKED BY NJS SCALE

1"=25'

EXIST. ELECTRIC METER EXIST. CABLE TV PEDESTAL

EXIST. FIBER OPTIC LINE

EXIST. OVERHEAD TELEPHONE LINE

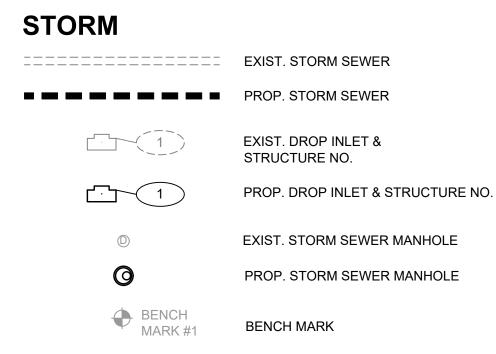
EXIST. OVERHEAD CABLE TV LINE

EXIST. ELECTRICAL TRANSFORMER

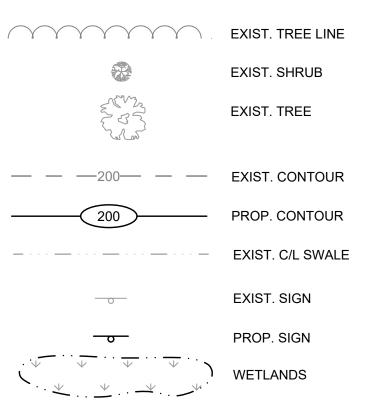
EXIST. UNDERGROUND CABLE TV LINE

EXIST. FIBER OPTIC BOX

— OCATV—



APPROX. BORING LOCATION EXIST. PAVED DITCH \bigcirc PROP. CLEARING LIMITS



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JOB NO. *41013.00*9 SHEET NO. 02A

4. AT LEAST 5 DAYS BEFORE BEING PLANTED. THE ENGINEER SHALL BE NOTIFIED THAT PLANTS ARE AVAILABLE FOR INSPECTION.

EROSION & SEDIMENT CONTROL NOTES

PROJECT DESCRIPTION

THE PURPOSE OF THIS PROJECT IS TO CONSTRUCT A NEW NORTH-SOUTH ROADWAY BETWEEN FAIRFAX BLVD./FARR AVE. AND ORCHARD STREET TO CREATE A GRID NETWORK WITHIN THE NORTHWEST QUADRANT OF FAIRFAX BLVD./CHAIN BRIDGE RD. (NORTHFAX). BIKE LANES, SIDEWALKS, AND PARKING LANES ARE PROPOSED TO BE CONSTRUCTED TO CONNECT EXISTING FACILITIES. THE ROADWAY IMPROVEMENTS WILL REQUIRE ACQUISITION OF RIGHT OF WAY AND PERMANENT AND TEMPORARY EASEMENTS. THE TOTAL LAND DISTURBANCE FOR THIS PROJECT IS APPROXIMATELY 1.49 ACRES.

EXISTING SITE CONDITIONS:

THE SURROUNDING TOPOGRAPHY FOR THE FARR AVE. EXTENSION PROJECT CONSISTS OF EXISTING PARKING LOTS AND COMMERCIAL ESTABLISHMENTS, WHICH WILL BE DEMOLISHED PRIOR TO CONSTRUCTION OF FARR AVENUE EXTENSION. NORTHERN PORTIONS OF THE PROPOSED PROJECT AREA ARE POCKETS OF WOODED AREAS AND RESIDENTIAL PROPERTIES. THE ENTIRE SURROUNDING AREA IS INTENDED TO BE RE-DEVELOPED ALONGSIDE THE CONSTRUCTION OF FARR AVENUE EXTENSION.

ADJACENT PROPERTY:

THE SURROUNDING PROPERTIES OF THE PROJECT SITE CONSIST OF VARIOUS COMMERCIAL AND RESIDENTIAL PROPERTIES, INCLUDING A CAR DEALERSHIP

THE PREDOMINANT SOIL FOR THIS PROJECT IS 95 - URBAN LAND. TOWARDS THE NORTHERN EXTENTS OF THE PROJECT, THE SOIL TYPE TRANSITIONS TO PRIMARILY 103A - WHEATON-CODORUS COMPLEX, 0 TO 2 PERCENT SLOPES, WITH SMALLER AREAS OF 105B - WHEATON-CLENELG COMPLEX, 2 TO 7 PERCENT SLOPES TO THE WEST OF THE ROAD. CONTRACTOR IS TO TAKE CAUTION IF ASBESTOS IS FOUND IN THE SOIL. SEE ENVIRONMENTAL NOTE #4 ON SHEET 2A FOR MORE DETAILS.

OFF-SITE AREAS:

NO OFF-SITE TOPSOIL STOCKPILE AREAS ARE NEEDED FOR THIS PROJECT TOPSOIL STOCKPILES WILL BE LOCATED AND DETERMINED BY THE CONTRACTOR.

CRITICAL EROSION AREAS

CRITICAL AREAS ARE AS FOLLOWS:

- ALL ADJACENT AND CONNECTING ROADS AND ENTRANCES FOR THE LENGTH OF THE PROJECT. CONTRACTOR MUST KEEP THE EXISTING ROAD FROM BUILD-UP OF SOIL.
- ADJACENT COMMERCIAL PROPERTIES
- SLOPES STEEPER THAN 3:1 SHALL BE PROTECTED WITH EC-2 MATTING TO PREVENT EROSION

EROSION AND SEDIMENT CONTROL MEASURES

UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICES SHALL BE CONSTRUCTED AND MAINTAINED ACCORDING TO THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE HANDBOOK. THE MINIMUM STANDARDS OF THE VESCH SHALL BE ADHERED TO UNLESS OTHERWISE WAIVED OR APPROVED BY A VARIANCE.

EROSION & SEDIMENT CONTROL SEQUENCE OF CONSTRUCTION:

- 1. SCHEDULE A PRECONSTRUCTION MEETING, GIVE A 48-HOUR NOTIFICATION OF THE PRECONSTRUCTION MEETING AND A CERTIFIED RESPONSIBLE LAND DISTURBER (CRLD) MUST BE PRESENT AT THE MEETING. VSMP OPERATIVE SHALL ATTEND THE PRECONSTRUCTION MEETING.
- PROVIDE MINIMAL CLEARING TO INSTALL SILT FENCE AND SAFETY FENCE AS SHOWN ON THE PLANS. INSTALL INLET PROTECTION AS REQUIRED FOR EXISTING INLETS. TAKE SPECIAL CARE TO KEEP SOIL BUILD-UP FROM ENTERING THE EXISTING ROAD PAVEMENT 3. THE CONTRACTOR WILL NOTIFY THE CITY INSPECTOR AT THE
- PRE-CONSTRUCTION MEETING OF THE PERMITTED LOCATION EXCESS SOIL MATERIAL IS HAULED TO OR BORROW MATERIAL IS BROUGHT IN FROM.
- 4. CLEAR AND GRUB AS INDICATED IN THE PLANS. MAINTAIN ACCESS ALONG EXISTING ROADS AT ALL TIMES.
- 5. TEMPORARY GRADING AND SEEDING IS REQUIRED WITHIN 7 DAYS OF DISTURBANCE FOR ALL AREAS WHICH ARE NOT TO BE ACTIVELY CONSTRUCTED UPON WITHIN 14 DAYS OF INITIAL DISTURBANCE.
- INSPECTIONS AND APPROVALS FOR COMPLETION OF INITIAL EROSION AND SEDIMENT CONTROL ACTIVITIES MUST BE RECEIVED BEFORE FURTHER CONSTRUCTION ACTIVITIES CAN BEGIN. ALL APPROVALS SHALL BE FROM THE CITY OF FAIRFAX.
- COMMENCE ROUGH GRADING AS REQUIRED FOR PROPOSED PAVEMENT
- 8. THE SITE SHALL BE PERMANENTLY STABILIZED AFTER ALL GRADING HAS
- BEEN COMPLETED BY SEEDING ALL DENUDED AREAS. INSTALL PROPOSED DRAINAGE STRUCTURES AND INSTALL INLET PROTECTION FOR PROPOSED FACILITIES AS DENOTED ON THE PLANS. MAINTAIN SILT FENCE AND SAFETY FENCE AT ALL TIMES AS DENOTED ON THE
- PLANS. 10. INSTALL STONE BASE FOR ROAD WIDENING AND ANY INTERMEDIATE AND/OR TOP COURSES OF ASPHALT ONCE FINAL GRADES ARE REACHED.
- 11. REFER TO PLAN SHEET XX FOR DETAILS REGARDING MAINTENANCE OF TRAFFIC.
- 12. UPON CONSTRUCTION COMPLETION, THE CONTRACTOR MUST CONTACT THE CITY OF FAIRFAX FOR EROSION CONTROL INSPECTION OF SLOPE STABILITY. EROSION CONTROL MEASURES MAY NOT BE REMOVED WITHOUT AUTHORIZATION BY THE CITY INSPECTOR.
- 13. ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUESTED BY THE CITY OF FAIRFAX AND/OR THE INSPECTOR AT ANY TIME DURING LAND DISTURBANCE.

MINIMUM STANDARDS

AN EROSION AND SEDIMENT CONTROL PROGRAM ADOPTED BY A DISTRICT OR LOCALITY MUST BE CONSISTENT WITH THE FOLLOWING CRITERIA, TECHNIQUES AND

- MS-1. PERMANENT OR TEMPORARY SOIL STABILIZATION SHALL BE APPLIED TO DENUDED AREAS WITHIN SEVEN DAYS AFTER FINAL GRADE IS REACHED ON ANY PORTION OF THE SITE. TEMPORARY SOIL STABILIZATION SHALL BE APPLIED WITHIN SEVEN DAYS TO DENUDED AREAS THAT MAY NOT BE AT FINAL GRADE BUT WILL REMAIN DORMANT FOR LONGER THAN 14 DAYS. PERMANENT STABILIZATION SHALL BE APPLIED TO AREAS THAT ARE TO BE LEFT DORMANT FOR MORE THAN ONE YEAR.
- MS-2. DURING CONSTRUCTION OF THE PROJECT, SOIL STOCKPILES AND BORROW AREAS SHALL BE STABILIZED OR PROTECTED WITH SEDIMENT TRAPPING MEASURES. THE APPLICANT IS RESPONSIBLE FOR THE TEMPORARY PROTECTION AND PERMANENT STABILIZATION OF ALL SOIL STOCKPILES ON SITE AS WELL AS BORROW AREAS AND SOIL INTENTIONALLY TRANSPORTED FROM THE PROJECT SITE.
- MS-3. A PERMANENT VEGETATIVE COVER SHALL BE ESTABLISHED ON DENUDED AREAS NOT OTHERWISE PERMANENTLY STABILIZED. PERMANENT VEGETATION SHALL NOT BE CONSIDERED ESTABLISHED UNTIL A GROUND COVER IS ACHIEVED THAT IS UNIFORM. MATURE ENOUGH TO SURVIVE AND WILL INHIBIT EROSION.
- MS-4. SEDIMENT BASINS AND TRAPS, PERIMETER DIKES, SEDIMENT BARRIERS AND OTHER MEASURES INTENDED TO TRAP SEDIMENT SHALL BE CONSTRUCTED AS A FIRST STEP IN ANY LAND-DISTURBING ACTIVITY AND SHALL BE MADE FUNCTIONAL BEFORE UPSLOPE LAND DISTURBANCE TAKES PLACE.
- MS-5. STABILIZATION MEASURES SHALL BE APPLIED TO EARTHEN STRUCTURES SUCH AS DAMS, DIKES AND DIVERSIONS IMMEDIATELY AFTER INSTALLATION.
- MS-6. SEDIMENT TRAPS AND SEDIMENT BASINS SHALL BE DESIGNED AND CONSTRUCTED BASED UPON THE TOTAL DRAINAGE AREA TO BE SERVED BY
 - A. THE MINIMUM STORAGE CAPACITY OF A SEDIMENT TRAP SHALL BE 134 CUBIC YARDS PER ACRE OF DRAINAGE AREA AND THE TRAP SHALL ONLY CONTROL DRAINAGE AREAS LESS THAN THREE ACRES
 - B. SURFACE RUNOFF FROM DISTURBED AREAS THAT IS COMPRISED OF FLOW FROM DRAINAGE AREAS GREATER THAN OR EQUAL TO THREE ACRES SHALL BE CONTROLLED BY A SEDIMENT BASIN. THE MINIMUM STORAGE CAPACITY OF A SEDIMENT BASIN SHALL BE 134 CUBIC YARDS PER ACRE OF DRAINAGE AREA THE OUTFALL SYSTEM SHALL, AT A MINIMUM, MAINTAIN THE STRUCTURAL INTEGRITY OF THE BASIN DURING A 25-YEAR STORM OF 24-HOUR DURATION. RUNOFF COEFFICIENTS USED IN RUNOFF CALCULATIONS SHALL CORRESPOND TO A BARE EARTH CONDITION OR THOSE CONDITIONS EXPECTED TO EXIST WHILE THE SEDIMENT BASIN IS UTILIZED.
- MS-7. CUT AND FILL SLOPES SHALL BE DESIGNED AND CONSTRUCTED IN A MANNER THAT WILL MINIMIZE EROSION. SLOPES THAT ARE FOUND TO BE ERODING EXCESSIVELY WITHIN ONE YEAR OF PERMANENT STABILIZATION SHALL BE PROVIDED WITH ADDITIONAL SLOPE STABILIZING MEASURES UNTIL THE PROBLEM IS CORRECTED.
- MS-8. CONCENTRATED RUNOFF SHALL NOT FLOW DOWN CUT OR FILL SLOPES UNLESS CONTAINED WITHIN AN ADEQUATE TEMPORARY OR PERMANENT CHANNEL, FLUME OR SLOPE DRAIN STRUCTURE.
- MS-9. WHENEVER WATER SEEPS FROM A SLOPE FACE, ADEQUATE DRAINAGE OR OTHER PROTECTION SHALL BE PROVIDED.
- MS-10. ALL STORM SEWER INLETS THAT ARE MADE OPERABLE DURING CONSTRUCTION SHALL BE PROTECTED SO THAT SEDIMENT-LADEN WATER CANNOT ENTER THE CONVEYANCE SYSTEM WITHOUT FIRST BEING FILTERED OR OTHERWISE TREATED TO REMOVE SEDIMENT.
- MS-11. BEFORE NEWLY CONSTRUCTED STORMWATER CONVEYANCE CHANNELS OR PIPES ARE MADE OPERATIONAL, ADEQUATE OUTLET PROTECTION AND ANY REQUIRED TEMPORARY OR PERMANENT CHANNEL LINING SHALL BE INSTALLED IN BOTH THE CONVEYANCE CHANNEL AND RECEIVING CHANNEL.
- MS-12. WHEN WORK IN A LIVE WATERCOURSE IS PERFORMED, PRECAUTIONS SHALL BE TAKEN TO MINIMIZE ENCROACHMENT, CONTROL SEDIMENT TRANSPORT AND STABILIZE THE WORK AREA TO THE GREATEST EXTENT POSSIBLE DURING CONSTRUCTION. NONERODIBLE MATERIAL SHALL BE USED FOR THE CONSTRUCTION OF CAUSEWAYS AND COFFERDAMS. EARTHEN FILL MAY BE USED FOR THESE STRUCTURES IF ARMORED BY NONERODIBLE COVER
- MS-13. WHEN A LIVE WATERCOURSE MUST BE CROSSED BY CONSTRUCTION VEHICLES MORE THAN TWICE IN ANY SIX-MONTH PERIOD. A TEMPORARY VEHICULAR STREAM CROSSING CONSTRUCTED OF NONERODIBLE MATERIAL SHALL BE PROVIDED.
- MS-14. ALL APPLICABLE FEDERAL, STATE AND LOCAL REGULATIONS PERTAINING TO WORKING IN OR CROSSING LIVE WATERCOURSES SHALL BE MET.
- MS-15. THE BED AND BANKS OF A WATERCOURSE SHALL BE STABILIZED IMMEDIATELY AFTER WORK IN THE WATERCOURSE IS COMPLETED.
- MS-16. UNDERGROUND UTILITY LINES SHALL BE INSTALLED IN ACCORDANCE WITH THE FOLLOWING STANDARDS IN ADDITION TO OTHER APPLICABLE CRITERIA: A. NO MORE THAN 500 LINEAR FEET OF TRENCH MAY BE OPENED AT ONE TIME.

B. EXCAVATED MATERIAL SHALL BE PLACED ON THE UPHILL SIDE OF

TRENCHES. C. EFFLUENT FROM DEWATERING OPERATIONS SHALL BE FILTERED OR PASSED THROUGH AN APPROVED SEDIMENT TRAPPING DEVICE, OR BOTH,

AND DISCHARGED IN A MANNER THAT DOES NOT ADVERSELY AFFECT

- FLOWING STREAMS OR OFF-SITE PROPERTY. D. MATERIAL USED FOR BACKFILLING TRENCHES SHALL BE PROPERLY COMPACTED IN ORDER TO MINIMIZE EROSION AND PROMOTE
- STABILIZATION. E. RESTABILIZATION SHALL BE ACCOMPLISHED IN ACCORDANCE WITH THESE
- F. APPLICABLE SAFETY REGULATIONS SHALL BE COMPLIED WITH.

MINIMUM STANDARDS (continued)

- MS-17. WHERE CONSTRUCTION VEHICLE ACCESS ROUTES INTERSECT PAVED OR PUBLIC ROADS, PROVISIONS SHALL BE MADE TO MINIMIZE THE TRANSPORT OF SEDIMENT BY VEHICULAR TRACKING ONTO THE PAVED SURFACE. WHERE SEDIMENT IS TRANSPORTED ONTO A PAVED OR PUBLIC ROAD SURFACE, THE ROAD SURFACE SHALL BE CLEANED THOROUGHLY AT THE END OF EACH DAY. SEDIMENT SHALL BE REMOVED FROM THE ROADS BY SHOVELING OR SWEEPING AND TRANSPORTED TO A SEDIMENT CONTROL DISPOSAL AREA STREET WASHING SHALL BE ALLOWED ONLY AFTER SEDIMENT IS REMOVED IN THIS MANNER. THIS PROVISION SHALL APPLY TO INDIVIDUAL DEVELOPMENT LOTS AS WELL AS TO LARGER LAND-DISTURBING ACTIVITIES
- MS-18. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED, UNLESS OTHERWISE AUTHORIZED BY THE LOCAL PROGRAM AUTHORITY. TRAPPED SEDIMENT AND THE DISTURBED SOIL AREAS RESULTING FROM THE DISPOSITION OF TEMPORARY MEASURES SHALL BE PERMANENTLY STABILIZED TO PREVENT FURTHER EROSION AND SEDIMENTATION.
- MS-19. PROPERTIES AND WATERWAYS DOWNSTREAM FROM DEVELOPMENT SITES SHALL BE PROTECTED FROM SEDIMENT DEPOSITION, EROSION AND DAMAGE DUE TO INCREASES IN VOLUME, VELOCITY AND PEAK FLOW RATE OF STORMWATER RUNOFF FOR THE STATED FREQUENCY STORM OF 24-HOUR DURATION IN ACCORDANCE WITH THE FOLLOWING STANDARDS AND
 - A. CONCENTRATED STORMWATER RUNOFF LEAVING A DEVELOPMENT SITE SHALL BE DISCHARGED DIRECTLY INTO AN ADEQUATE NATURAL OR MAN-MADE RECEIVING CHANNEL, PIPE OR STORM SEWER SYSTEM. FOR THOSE SITES WHERE RUNOFF IS DISCHARGED INTO A PIPE OR PIPE SYSTEM, DOWNSTREAM STABILITY ANALYSIS AT THE OUTFALL OF THE PIPE OR PIPE SYSTEM SHALL BE PERFORMED.
 - ADEQUACY OF ALL CHANNELS AND PIPES SHALL BE VERIFIED IN THE FOLLOWING MANNER:
 - 1. THE APPLICANT SHALL DEMONSTRATE THAT THE TOTAL DRAINAGE AREA TO THE POINT OF ANALYSIS WITHIN THE CHANNEL IS ONE HUNDRED TIMES GREATER THAN THE CONTRIBUTING DRAINAGE AREA OF THE PROJECT IN QUESTION.
 - 2. NATURAL CHANNELS SHALL BE ANALYZED BY THE USE OF A TWO-YEAR FREQUENCY STORM TO VERIFY THAT THE STORMWATER WILL NOT OVERTOP CHANNEL BANKS NOR CAUSE EROSION OF CHANNEL BED OR BANKS.
 - 3. ALL PREVIOUSLY CONSTRUCTED MAN-MADE CHANNELS SHALL BE ANALYZED BY THE USE OF A TEN-YEAR FREQUENCY STORM TO VERIFY THAT STORMWATER WILL NOT OVERTOP ITS BANKS AND BY THE USE OF A TWO-YEAR STORM TO DEMONSTRATE THAT STORMWATER WILL BE CONTAINED WITHIN THE PIPE OR SYSTEM.
 - 4. PIPES AND STORM SEWER SYSTEMS SHALL BE ANALYZED BY THE USE OF A TEN-YEAR FREQUENCY STORM TO VERIFY THAT THE STORMWATER WILL BE CONTAINED WITHIN THE PIPE OR SYSTEM.
 - C. IF EXISTING NATURAL RECEIVING CHANNELS OR PREVIOUSLY CONSTRUCTED MAN-MADE CHANNELS OR PIPES ARE NOT ADEQUATE.
 - THE APPLICANT SHALL: 1. IMPROVE THE CHANNELS TO A CONDITION WHERE A TEN-YEAR FREQUENCY STORM WILL NOT OVERTOP THE BANKS AND A TWO-YEAR FREQUENCY STORM WILL NOT CAUSE EROSION TO THE CHANNEL, BED OR BANKS; OR
 - 2. IMPROVE THE PIPE OR PIPE SYSTEM TO A CONDITION WHERE THE TEN-YEAR FREQUENCY STORM IS CONTAINED WITHIN THE APPURTENANCES: OR
 - 3. DEVELOP A SITE DESIGN THAT WILL NOT CAUSE THE PRE-DEVELOPMENT PEAK RUNOFF RATE FROM A TWO-YEAR STORM TO INCREASE WHEN RUNOFF OUTFALLS INTO A NATURAL CHANNEL OR WILL NOT CAUSE THE PRE-DEVELOPMENT PEAK RUNOFF RATE FROM A TEN-YEAR STORM TO INCREASE WHEN RUNOFF OUTFALLS INTO A MAN-MADE CHANNEL
 - 4. PROVIDE A COMBINATION OF CHANNEL IMPROVEMENT STORMWATER DETENTION/RETENTION OR OTHER MEASURES WHICH IS SATISFACTORY TO THE PLAN APPROVING AUTHORITY TO PREVENT DOWNSTREAM EROSION.
 - D. THE APPLICANT SHALL PROVIDE EVIDENCE OF PERMISSION TO MAKE THE IMPROVEMENTS.
 - E. ALL HYDROLOGICAL ANALYSIS SHALL BE BASED ON THE EXISTING WATERSHED CHARACTERISTICS AND THE ULTIMATE DEVELOPMENT CONDITION OF THE SUBJECT PROJECT.
 - F. IF THE APPLICANT CHOOSES AN OPTION THAT INCLUDES STORMWATER DETENTION/RETENTION, HE SHALL OBTAIN APPROVAL FROM THE LOCALITY OF A PLAN FOR MAINTENANCE OF THE DETENTION FACILITIES. THE PLAN SHALL SET FORTH THE MAINTENANCE REQUIREMENTS OF THE FACILITY AND THE PERSON RESPONSIBLE FOR PERFORMING THE MAINTENANCE.
 - G. INCREASED VOLUMES OF SHEET FLOWS THAT MAY CAUSE EROSION OR SEDIMENTATION ON ADJACENT PROPERTY SHALL BE DIVERTED TO A STABLE OUTLET, ADEQUATE CHANNEL OR DETENTION FACILITY.
 - H. IN APPLYING THESE STORMWATER MANAGEMENT CRITERIA, INDIVIDUAL LOTS IN A RESIDENTIAL SUBDIVISION DEVELOPMENT SHALL NOT BE CONSIDERED TO BE SEPARATE DEVELOPMENT PROJECTS. INSTEAD, THE RESIDENTIAL SUBDIVISION DEVELOPMENT, AS A WHOLE, SHALL BE CONSIDERED TO BE A SINGLE DEVELOPMENT PROJECT. HYDROLOGIC PARAMETERS THAT REFLECT THE ULTIMATE SUBDIVISION DEVELOPMENT SHALL BE USED IN ALL ENGINEERING CALCULATIONS
 - PROPOSED COMMERCIAL OR INDUSTRIAL SUBDIVISIONS SHALL APPLY THESE STORMWATER MANAGEMENT CRITERIA TO THE DEVELOPMENT AS A WHOLE, HYDROLOGIC PARAMETERS THAT REFLECT THE ULTIMATE SUBDIVISION DEVELOPMENT SHALL BE USED IN ALL ENGINEERING CALCULATIONS.

ADDITIONAL NOTES:

 CONTRACTOR MUST KEEP ALL EXISTING ROADS FREE FROM THE BUILDUP OF SOIL, SEDIMENT, AND DEBRIS

2. SHOULD THE CONTRACTOR DETERMINE THAT A STOCKPILE AREA IS REQUIRED. THE LOCATION AND LIMITS OF THE STOCKPILE AREA MUST BE SELECTED BY THE CONTRACTOR. IF THE AREA IS OUTSIDE THE LIMITS OF DISTURBANCE AS SHOWN ON THESE PLANS, THE LOCATION MUST BE SUBMITTED TO AND APPROVED BY THE CITY OF FAIRFAX PRIOR TO CONSTRUCITON.

3. THE CONTRACTOR WILL PROVIDE THE LOCATION THAT EXCESS SOIL IS HAULED TO, OR BORROW MATERIAL IS SOURCESD FROM, TO THE ENVIRONMENTAL ENGINEERING INSPECTOR AT THE PRE-CONSTRUCTION MEETING.

4. ANY ADDITIONAL PERMITTING FOR OFFSITE STOCKPILES, BORROW SOURCES, OR STAGING OF MEN/EQUIPMENT (IF REQUIRED) SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

EROSION & SEDIMENT CONTROL QUANTITIES

TEMPORARY SILT FENCE 1770 LF TEMPORARY SAFETY FENCE 750 LF LIMITS OF DISTURBANCE 1.49 AC **INLET PROTECTION TYPE B** 12 EA SILTATION CONTROL EXCAVATION 50 CY 0.72 AC TEMPORARY SEEDING PERMANENT SEEDING 0.72 AC

EROSION & SEDIMENT CONTROL LEGEND								
	SAF TEMPORARY SAFETY FENCE, VESCH ST'D. 3.01							
	SF-B TEMPORARY SILT FENCE, VDOT ST'D. EC-5, TYPE B							
	IP-B INLET PROTECTION, PER VDOT ST'D. EC-6 TYPE B							
	CE CONSTRUCTION ENTRANCE VESCH ST'D. 3.02							

PLANS SUBJECT TO CHANGE

INFORMATION ONLY

NOT FOR CONSTRUCTION

04/28/2020 DRAWN BY

DESIGNED BY CHECKED BY

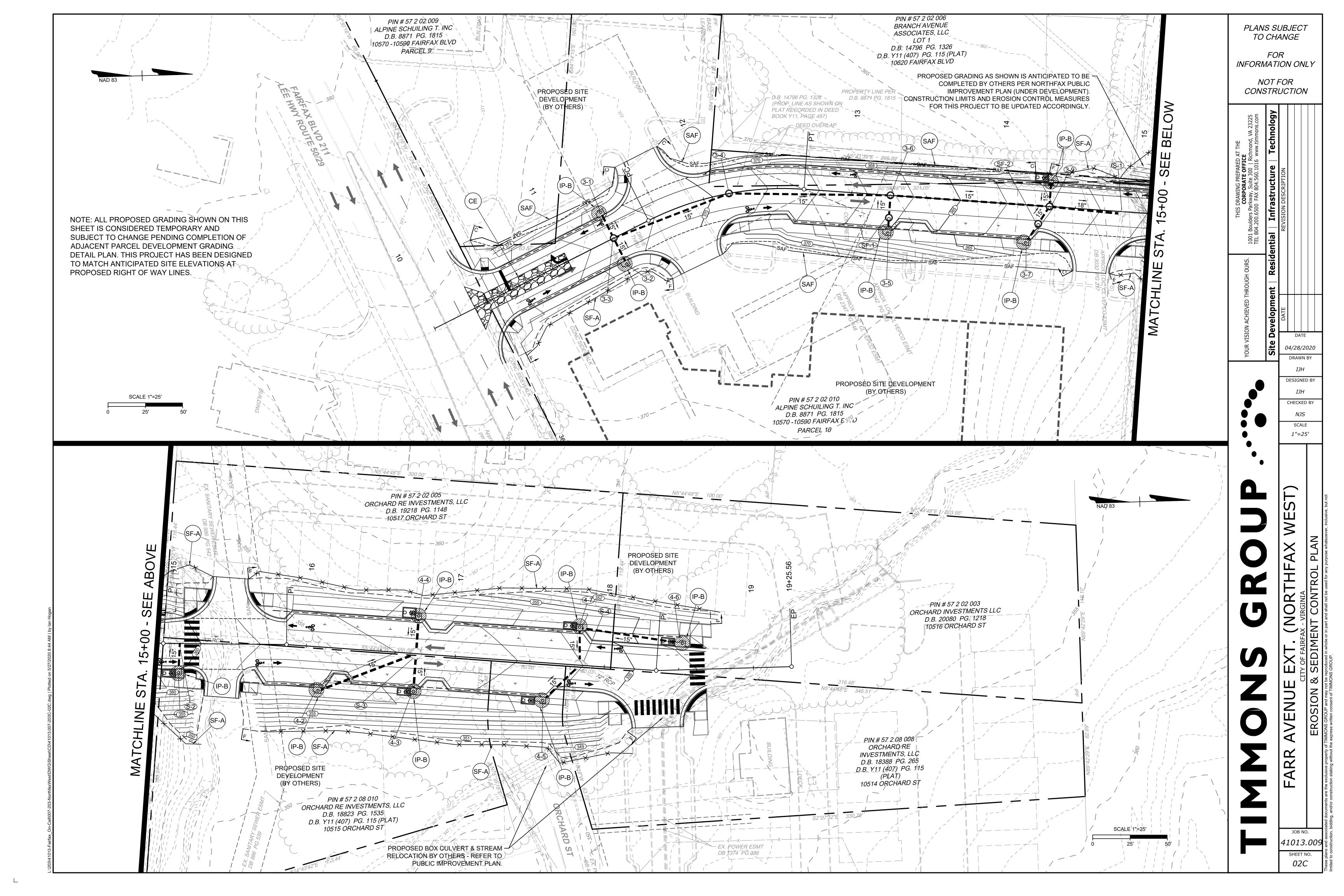
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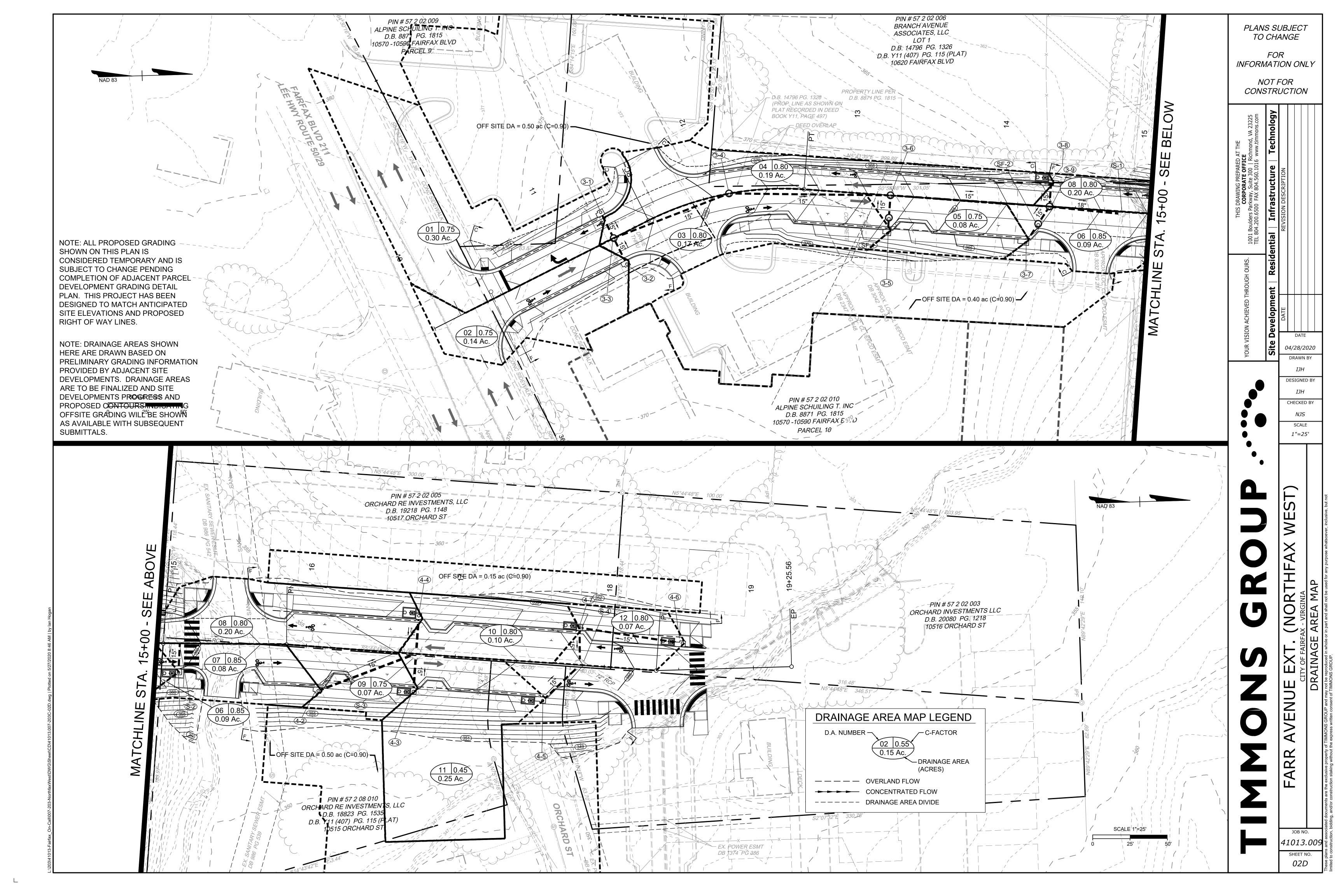
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ROADSIDE DEVELOPMENT

CORE MIX

MIX	LBS./ ACRES	DESCRIPTION				
1	200					
2		100% CERTIFIED TALL FESCUE				
3	100	50 % CERTIFIED TALL FESCUE				
4		50% ORCHARDGRASS				
4		50% CERTIFIED KENTUCKY BLUEGRASS				
5		100% BERMUDAGRASS				
TEMPORARY						
3/1 - 5/16 and	50	50% CERTIFIED TALL FESCUE				
8/16 - 3/1	50	50% BARLEY, WINTER RYE OR WINTER WHEAT				
5/16 - 8/16	50	50% FOXTAIL MILLET				
3/10 - 0/10	50	50% CERTIFIED TALL FESCUE				

ALL RATES TO BE SPECIFIED BY THE DISTRICT ROADSIDE MANAGER

* FINE FESCUES INCLUDE CHEWINGS, CREEPING RED. HARD, SHEEP

ADDITIVES

TYPE	LBS./ ACRES	DESCRIPTION
А		100% LOVEGRASS
В	20	100% BARLEY, WINTER RYE OR WINTER WHEAT
С	10	100% FOXTAIL MILLET
D	10	100% ANNUAL RYEGRASS
E		100% CROWNVETCH (LEGUME)
F		100% SERICEA LESPEDEZA (LEGUME)
G		100% BIRDSFOOT TREFOIL (LEGUME)
Н	10	Little Bluestem
	A	
J	A	
K	A	

SECTION OF SEED LOCATIONS

PEATER	FLATTER than 3:1	FLATTER than 3 : 1	3:1 or GREATER
3:1 or GREATER (SLOPE)	MOWED	MOWED	3:101 (SLOPE)

SEEDING SCHEDULE

	ROADSIDE DEVELOPMENT SUMMARY									
PROJECT NUMBERS	TOPSOIL 2" CLASS A B	REGULAR SEED	OVER SEEDING	LIME	FERT. 15-30-15	LEGUME SEED	LEGUME OVER SEEDING	TEMPORARY SEEDING		
	ACRES	LBS.	LBS.	TONS	TONS	LBS.	LBS.	LBS.		

DENOTES ITEM/	S) TO BE DAID FOR	AN DAGIC OF	DI ANI OLIANITI	TIEC IN
DENOTES ITEM(S) TO BE PAID FOR (JIN DASIS UF	PLAN QUANTI	HEO III
ACCOPDANCE I	WITH CURRENT ROA	U VND BDID	CE ODECIEICA	TIONS
ACCORDANCE I	MITH CORRENT ROP	יטואם טווא טא	JE SPECIFICA	HONS.

	SLOPES SEED MIX WITH ADDITIVE	MOWED SEED MIX WITH ADDITIVE	SLOPES SEED MIX WITH ADDITIVE	MOWED SEED MIX WITH ADDITIVE	SLOPES SEED MIX WITH ADDITIVE	MOWED SEED MIX WITH ADDITIVE	
	SPRI MONTH 8 4/1 - 6	& DATE	SUM MONTH 6/1 -		FALL & WINTER MONTH & DATE 9/15 - 4/1		
PROJECT NUMBERS URBAN	1D	1D	1C	1C	1B	1B	
STANDARD	3D	3D	3CH	3C	3B	3B	
X SPECIFY KIND OF FINE FESCUE	HARD	HARD	HARD	HARD	HARD	HARD	

NOTES:

APPROXIMATELY 0.72 ACRES WILL BE DISTURBED ON THIS PROJECT AND WILL REQUIRE THE ESTABLISHMENT OF GRASSES AND/OR LEGUMES.

NOTES FOR FIELD USE ONLY

OVERSEEDING RATES SHALL BE 100 PERCENT OF THE SEED MIXTURE SUPPLIED WITHOUT FERTILIZER.

THE ENGINEER WILL REQUIRE THE CONTRACTOR TO PERFORM SUPPLEMENTAL SEEDING WHEN LESS THAN 75 PERCENT UNIFORM STAND OF THE PERMANENT GRASS SPECIFIED IN THE MIXTURES IS OBTAINED. (ANNUAL SPECIES SUCH AS, RYE AND MILLET ARE TEMPORARY VARIETIES AND REQUIRE SUPPLEMENTAL SEEDING.)

NOTES APPLY TO SCHEDULE

LEGUME SEED MIXES (BIRDSFOOT TREFOIL, CROWNVETCH, AND SERICEA LESPEDEZA) AND WEEPING LOVEGRASS SHALL NOT BE USED ON SHOULDERS AND OTHER LOCATIONS FLATTER THAN 3:1 SLOPE.

LEGUME SEED SHALL BE INOCULATED WITH THE APPROPRIATE STRAIN AND RATE OF BACTERIA. FOR HYDROSEEDING, USE FIVE TIMES THE DRY SEEDING RATE OF INOCULATE.

A TEMPORARY MIX OR EROSION CONTROL MULCH, AS DIRECTED BY THE ENGINEER, IS TO BE USED ONLY ON AREAS THAT ARE TO BE REGRADED OR LATER DISTURBED, IF LEFT DORMANT FOR MORE THAN 15 DAYS.

EROSION CONTROL MULCH, AS DIRECTED BY THE ENGINEER, IS TO BE USED ON AREAS THAT ARE TO BE LEFT DORMANT FOR MORE THAN 15 DAYS BETWEEN DECEMBER 1 AND FEBRUARY 28.

EROSION CONTROL MULCH, AS LISTED ON THE VDOT APPROVED PRODUCTS LIST, SHALL BE APPLIED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.

SPRING & SUMMER AND FALL & WINTER DEFINED FOR THE PURPOSE OF DETERMINING WHETHER HULLED OR UNHULLED BERMUDAGRASS AND SERICEA LESPEDEZA SEED IS REQUIRED:

EROSION CONTROL MULCH SHALL PROVIDE 100 PERCENT COVERAGE OF ALL DENUDED AREAS.

SPRING & SUMMER 4/1 - 9/15 - USE HULLED SEED FALL & WINTER 9/15 - 4/1 - USE UNHULLED SEED

TYPEIMULCH (STRAW) TO BE USED ON NEWLY SEEDED AREAS ADJACENT TO ALL WATERWAYS, WETLANDS, SWAMPS, OR ANY AREA IN WHICH DRAINAGE FLOWS TOWARD AREAS UNDER THE JURISDICTION OF THE ENVIRONMENTAL REGULATORY AGENCIES.

TYPE I MULCH SHALL BE APPLIED TO PROVIDE A MINIMUM 90 PERCENT COVERAGE.

TYPE I MULCH SHALL BE TACKED WITH FIBER MULCH AT THE RATE OF 750 LBS. PER ACRE AND/OR MULCH TACKIFIER.

TYPE I MULCH (FIBER MULCH) MAY BE SUBSTITUTED FOR TYPE MULCH AT THE RECOMMENDATION OF THE DISTRICT ROADSIDE MANAGER.

TYPE I MULCH SHALL BE APPLIED AT A RATE OF 1500 LBS. (NET DRY WEIGHT) PER ACRE TO PROVIDE A MINIMUM OF 90 PERCENT COVERAGE, AND SHALL BE APPLIED IN A SEPARATE APPLICATION.

ALL TOPSOIL IS TO BE FREE OF HARD LUMPS, CLODS, ROCKS AND FOREIGN DEBRIS AND IS TO BE HAND RAKED TO TIE INTO EXISTING LAWNS.

ALL SEED MUST BE IN CONFORMANCE WITH VDOT SEED SPECIFICATIONS FOR GRASSES & LEGUMES AND BE PROVIDED AT THE PROJECT SITE IN BAGS NOT OPENED AND LABELED FOR USE ON VDOT PROJECTS WITH A GREEN TAG CERTIFYING INSPECTION BY THE VIRGINIA CROP IMPROVEMENT ASSOCIATION.

MIX REQUIREMENTS THIS PROJECT

- MIX 1 (URBAN) SHALL BE USED FOR COMMERCIAL/RESIDENTIAL/CHURCH PROPERTIES TO MORE CLOSELY MATCH THOSE TYPICALLY MORE MANICURED LAWNS. MIX 3 (STANDARD) IS TO BE USED WHERE MIX 1 IS NOT

- THREE (3) INCHES OF TOPSOIL SHALL BE USED WHENEVER MIX 1 IS USED, AND FOR ANY SHOULDER/DITCH/MEDIAN/STORMWATER MANAGEMENT AREA (MIX 3). EC-2 EROSION CONTROL MATTING SHALL BE USED WHEN MIX 1 IS USED IN FRONT OF COMMERCIAL/RESIDENTIAL/CHURCH PROPERTIES, WHERE THE SLOPE WILL BE STEEPER THAN 3:1.

PLANS SUBJECT TO CHANGE

INFORMATION ONLY

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04/28/2020

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1"=25'

41013.009

SHEET NO.

TRANSPORTATION MANAGEMENT PLAN

GENERAL:

- 1. THIS PROJECT IS CLASSIFIED AS TYPE A. CATEGORY II.
- 2. THE PROJECT LENGTH ALONG THE PROPOSED ALIGNMENT IS APPROXIMATELY 850 FEET. THE WIDTH OF THE WORK ZONE IS APPROXIMATELY 80 FEET IN MOST AREAS.
- 3. THE PURPOSE OF THIS PROJECT IS TO CONSTRUCT A NEW NORTH-SOUTH ROADWAY BETWEEN FAIRFAX BLVD./FARR AVE. AND ORCHARD STREET TO CREATE A GRID NETWORK WITHIN THE NORTHWEST QUADRANT OF FAIRFAX BLVD./CHAIN BRIDGE RD. (NORTHFAX). BIKE LANES, SIDEWALKS, AND PARKING LANES ARE PROPOSED TO BE CONSTRUCTED TO CONNECT EXISTING FACILITIES.
- 4. TRAFFIC ALONG THE NEARBY ROADS CONSISTS OF COMMUTERS AND LOCAL RESIDENTS.
- 5. THE EXISTING SPEED LIMITS FOR FAIRFAX BOULEVARD AND ORCHARD STREET ARE 35 MPH AND 25 MPH, RESPECTIVELY. ALL EXISTING SPEED LIMITS WILL BE MAINTAINED DURING ALL PHASES OF CONSTRUCTION.

TEMPORARY TRAFFIC CONTROL (TTC) / MAINTENANCE OF TRAFFIC (MOT):

- 1. LANE CLOSURES AND SHOULDER CLOSURES ARE ANTICIPATED FOR THIS PROJECT AND WILL BE IN ACCORDANCE WITH THE VIRGINIA WORK AREA PROTECTION MANUAL, 2011 EDITION, (SEPTEMBER, 2019 REVISION). THE FOLLOWING TEMPORARY TRAFFIC MEASURES SHALL BE USED:
- 1.1. TYPICAL TRAFFIC CONTROL STATIONARY OPERATION ON SHOULDER (FIGURE TTC-4.2 -STATIONARY OPERATION ON A SHOULDER)
- 1.2. TYPICAL TRAFFIC CONTROL SHOULDER OPERATION WITH MINOR ENCROACHMENT (FIGURE TTC-5.2 - SHOULDER OPERATION WITH MINOR ENCROACHMENT)
- 1.3. TYPICAL TRAFFIC CONTROL FOR OUTSIDE LANE CLOSURE OPERATION (FIGURE TTC-16.2 - OUTSIDE LANE CLOSURE OPERATION).
- 2. ALL DIMENSIONS SHOWN FOR SIGN PLACEMENT SHALL BE PROVIDED TO THE MAXIMUM EXTENT PRACTICAL SHOWN IN THE APPROPRIATE TTC FIGURES. SHOULD ADJUSTMENTS BE REQUIRED. THE CONTRACTOR SHALL OBTAIN APPROVAL FROM THE ENGINEER PRIOR TO SIGN PLACEMENT.
- 3. NEGATIVE IMPACTS TO THE TRAVELING PUBLIC SHALL BE MINIMIZED IN EVERY WAY POSSIBLE. CONTRACTOR SHALL AVOID, TO THE BEST OF ITS ABILITY, LANE CLOSURES AND OTHER SIGNIFICANT DISRUPTIONS TO TRAFFIC DURING PEAK HOURS AND HOLIDAYS.
- 8. ACCESS TO ADJACENT PROPERTIES SHALL BE MAINTAINED AT ALL TIMES. IF THE CONTRACTOR DETERMINES A CLOSURE OF A ROAD IS NECESSARY, HE/SHE SHALL BE RESPONSIBLE FOR OBTAINING APPROVALS FROM THE ENGINEER. CITY OF FAIRFAX AND VDOT. NO CLOSURES ARE PERMITTED WITHOUT PRIOR WRITTEN PERMISSION. THE CONTRACTOR SHALL COORDINATE WITH ADJACENT USERS AND/OR CONTRACTORS TO **ENSURE ADEQUATE ACCESS IS MAINTAINED AT ALL TIMES**

PUBLIC COMMUNICATION PLAN:

- 1. IF ANY MAJOR TRAFFIC CHANGES ARE TO TAKE PLACE (LANE CLOSURES, ETC.), THE CONTRACTOR SHALL NOTIFY THE CITY OF FAIRFAX TRANSPORTATION DEPARTMENT WHO WILL ENSURE THAT THE LOCAL NEWSPAPERS AND RADIO STATIONS ARE INFORMED 72 HOURS IN ADVANCE OF THE CHANGE.
- 2. PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) SHALL BE USED TO NOTIFY THE TRAVELING PUBLIC OF ANY SUCH MAJOR TRAFFIC CHANGES 72 HOURS IN ADVANCE OF THE CHANGE. PCMS SHALL REMAIN IN PLACE 48 HOURS AFTER CHANGE IS IMPLEMENTED.

TRANSPORTATION OPERATION PLAN:

- 1. THE PROJECT PERSONNEL SHALL NOTIFY THE "NORTHERN REGION OPERATION TRAFFIC OPERATIONS CENTER" (NROTOC), WHEN A LANE CLOSURE IS TO BE IMPLEMENTED, AND AGAIN WHEN IT IS REMOVED. THE NROTOC WILL IN TURN PLACE THE INFORMATION INTO THE "511 VIRGINIA" TRAFFIC ALERT SYSTEM (A SERVICE OF THE VIRGINIA DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEM).
- 2. THE CONTRACTOR SHALL USE THE VDOT LCAMS (LANE CLOSURES ADVISORY MANAGEMENT SYSTEM) FOR SCHEDULING LANE CLOSURES. THE CONTRACTOR SHALL INPUT PLANNED LANE CLOSURES FOR THE UPCOMING WEEK INTO THE LCAMS SYSTEM AND SUBMIT (VIA EMAIL OR FAX) THE LCAMS TICKET NUMBER TO THE AREA CONSTRUCTION ENGINEER AND PROJECT PERSONNEL BY 12:00 PM ON TUESDAY OF THE WEEK PRIOR TO PLANNED LANE CLOSURE OPERATIONS
- 3. THE FOLLOWING IS A LIST OF CONTACT NUMBERS **EMERGENCY: 911** FIRE/RESCUE:703-385-7940 CITY OF FAIRFAX POLICE: 703-385-7940 STATE POLICE: 703-803-2660 ENGINEER CONTACT: NICK SOUCIE, TIMMONS GROUP: 804-200-6431 NORTHERN REGIONAL TRAFFIC OPERATIONS CENTER: 703-554-6712 PUBLIC AFFAIRS: JENNI McCORD, 703-259-1779 VDOT NORTHERN VIRGINIA DISTRICT: 800-367-7623 DISTRICT WORK ZONE SAFETY COORDINATOR: GEOF SARMAC, 703-259-1985 CITY OF FAIRFAX: WENDY SANFORD, TRANSPORTATION DIRECTOR, 703-385-7889
- 4. ANY TRAFFIC INCIDENT THAT OCCURS DURING THE LIFE OF THIS PROJECT WILL BE DISCUSSED BY THE CONTRACTOR, ENGINEER, VDOT PERSONNEL (AS REQUIRED), AND THE CITY PROJECT PERSONNEL TO DETERMINE WHETHER ANY CHANGES NEED TO BE MADE TO THE TRAFFIC CONTROL ON THE PROJECT.

PEDESTRIAN MANAGEMENT

1. PEDESTRIAN WALKWAYS (AS APPLICABLE) SHALL BE CLOSED AS NECESSARY TO ENSURE A SAFE ENVIRONMENT FOR PEDESTRIANS DURING CONSTRUCTION. CLOSED WALKWAYS SHALL BE MARKED WITH SIGNS AND OPTIC-ORANGE SAFETY FENCE (AS NECESSARY). CONTRACTOR SHALL PROVIDE AN ADEQUATE, SAFE, ADA ACCESSIBLE ALTERNATE ROUTE FOR ALL WALKWAY CLOSURES.

TRANSPORTATION MANAGEMENT PLAN (TMP) GENERAL NOTES:

- 1. ANY REQUIRED LANE CLOSURES MUST BE APPROVED IN ADVANCE BY THE CITY OF FAIRFAX TRANSPORTATION DEPARTMENT.
- 2. ALLOWABLE WORK HOURS SHALL BE LIMITED TO BETWEEN THE HOURS OF 7:00 AM TO 6:00 PM MONDAY-FRIDAY AND 8:30 AM TO 5:00 PM ON SATURDAYS, UNLESS OTHERWISE DIRECTED BY THE DIRECTOR OF PUBLIC WORKS. ALLOWABLE LANE CLOSURE HOURS SHALL BE LIMITED TO BETWEEN THE HOURS OF 9:00 AM TO 3:00 PM. UNLESS OTHERWISE APPROVED BY THE CITY OF FAIRFAX TRANSPORTATION DIRECTOR.
- 3. TRAFFIC SHALL NOT BE STOPPED ON FAIRFAX BOULEVARD (ROUTE 29) OR ORCHARD STREET FOR LONGER THAN FIVE MINUTES AT ANY TIME UNLESS OTHERWISE APPROVED BY THE CITY.
- 4. ALL AREAS EXCAVATED DEEPER THAN TWO INCHES (2") BELOW EXISTING PAVEMENT SURFACE AND WITHIN THE CLEAR ZONE. AT THE CONCLUSION OF EACH WORKDAY. SHALL BE BACK FILLED TO FORM AN APPROXIMATE 6:1 WEDGE AGAINST THE PAVEMENT SURFACE FOR THE SAFETY AND PROTECTION OF VEHICULAR TRAFFIC. ALL COST FOR PLACING, MAINTAINING AND REMOVING THE 6:1 WEDGE SHALL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS IN THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- 5. LANE CLOSURES WILL NOT BE PERMITTED ON HOLIDAYS OR WEEKENDS UNLESS OTHERWISE APPROVED IN ADVANCE BY THE DIRECTOR OF PUBLIC WORKS
- 6. ANY CONTRACT ITEM(S) NOT SPECIFICALLY NOTED IN THE MAINTENANCE OF TRAFFIC MAY BE SCHEDULED FOR CONSTRUCTION AT THE CONTRACTOR'S OPTION, AS APPROVED BY THE ENGINEER, THE CITY, AND VDOT.
- 7. VEHICULAR AND PEDESTRIAN ACCESS TO ADJACENT PROPERTIES AND CONNECTING STREETS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. PROPERTY OWNERS SHALL BE NOTIFIED A MINIMUM OF 48 HOURS PRIOR TO ANY ACTIVITIES WHICH WOULD IMPEDE ACCESS TO THEIR PROPERTY. IN ALL CASES IN WHICH EXISTING OR ESTABLISHED TRAFFIC PATTERNS WILL BE DISRUPTED, THE CONTRACTOR WILL NOTIFY ALL AFFECTED RESIDENTS AND/OR BUSINESSES A MINIMUM OF 48 HOURS IN ADVANCE OF THE ANTICIPATED DISRUPTION BY DISTRIBUTING DOOR-TO-DOOR NOTICES. A COPY OF THE NOTICE SHALL BE FORWARDED TO THE PROJECT ENGINEER, THE CITY, AND/OR VDOT FOR REVIEW AND APPROVAL PRIOR TO THE BEGINNING OF WORK.
- 8. THE FINAL SURFACE COURSE IS NOT TO BE PLACED UNTIL SUCH TIME THAT PERMANENT PAVEMENT MARKINGS CAN BE PLACED.
- 9. ALL TRAFFIC CONTROL SHALL BE SET UP AND SPACED ACCORDING TO THE VIRGINIA WORK AREA PROTECTION MANUAL, 2011 EDITION, (SEPTEMBER, 2019 REVISION).
- 10. CONTRACTOR SHALL PROVIDE ADDITIONAL TRAFFIC CONTROL AS DIRECTED BY THE CITY AND/OR VDOT SHOULD FIELD CONDITIONS WARRANT.
- 11. CONTRACTOR MAY REDUCE LANE WIDTHS TO 10' MINIMUM DURING CONSTRUCTION. ANY TEMPORARY PAVEMENT MARKINGS THAT ARE REQUIRED ARE THE RESPONSIBILITY OF THE CONTRACTOR.
- 12. CONTRACTOR IS RESPONSIBLE FOR PLACEMENT AND MAINTENANCE OF ALL TEMPORARY PAVEMENT MARKINGS THAT ARE REQUIRED OR IMPLIED IN THE TTC DIAGRAMS. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING ALL TRAFFIC CONTROL DEVICES, SIGNAGE, EQUIPMENT, PERSONNEL, INCLUDING CERTIFIED TRAFFIC CONTROL PERSONNEL, ETC. TO CONTROL TRAFFIC DURING CONSTRUCTION WITHIN VDOT AND/OR CITY MAINTAINED RIGHT-OF-WAY. ALL TRAFFIC CONTROL SHALL BE IN STRICT ACCORDANCE WITH THE STANDARDS, GUIDELINES, POLICIES, AND OBJECTIVES OF THE LATEST EDITION OF THE VIRGINIA WORK AREA PROTECTION MANUAL, MANUAL FOR UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), AND ALL CITY AND/OR VDOT PERMITS
- 13. AT NO TIME SHALL CONSTRUCTION TAKE PLACE ON BOTH THE RIGHT AND LEFT SIDES OF VEHICLES UNLESS SPECIFIED BY THE CITY AND/OR VDOT.
- 14. ALL CONSTRUCTION MATERIALS AND EQUIPMENT SHALL BE STORED OFF-SITE OR OUTSIDE OF CLEAR ZONE/SIGHT DISTANCE RESTRICTIVE AREAS.
- 15. EMERGENCY VEHICLE. BUS. AND ALL MAIL ROUTES ACCESS SHALL BE MAINTAINED AT ALL TIMES.

SEQUENCE OF CONSTRUCTION (SOC):

THE FOLLOWING SEQUENCE OF CONSTRUCTION IS A RECOMMENDATION PROVIDED BY THE ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING APPROVAL FOR ANY CHANGES TO THIS PLAN RESULTING FROM ADJUSTMENTS TO THE RECOMMENDED SEQUENCING. ALL PHASES OF CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE EROSION AND SEDIMENT CONTROL PLANS PROVIDED IN THIS PLAN SET. THE GENERAL SEQUENCE SHALL BE TO CONSTRUCT THE IMPROVEMENTS ALONG THE NEW ROAD ALIGNMENT OUTSIDE OF EXISTING TRAFFIC PATTERNS TO THE GREATEST EXTENT PRACTICAL, AND THEN TO MAKE FINAL CONNECTIONS TO THE EXISTING ROADWAYS AS A SECOND PHASE OF WORK

PHASE 1

TRAFFIC SHALL BE MAINTAINED IN GENERAL ACCORDANCE WITH TTC-4.2 AND TTC 5.2 DURING THIS PHASE OF CONSTRUCTION, AS PRESCRIBED IN THE GENERAL NOTES, TTC/MOT NOTES, AND THROUGH THE USE OF GROUP II CHANNELIZING DEVICES AND PROPER SIGNAGE. ADJACENT ENTRANCES ALONG ORCHARD STREET AND FAIRFAX BOULEVARD SHALL REMAIN OPEN TO VEHICULAR TRAFFIC DURING CONSTRUCTION. THIS WORK WILL PRIMARILY TAKE PLACE OUTSIDE OF AND BETWEEN THE CORRIDORS OF FAIRFAX BOULEVARD AND ORCHARD STREET, AND THEREFORE LARGELY OUTSIDE OF LIVE TRAFFIC.

- 1-1 INSTALL ALL SIGNING FOR PROJECT LIMITS: COMMENCE CLEARING, SITE PREPARATION, AND INSTALLATION OF EROSION CONTROL MEASURES IN ACCORDANCE WITH THE PROVIDED PLAN. ALL DEMOLITION, REMOVAL, OR ADJUSTMENTS OF EXISTING FACILITIES SHALL BE COMPLETED. INITIAL CONSTRUCTION CONSISTS OF CLEARING, GRUBBING, AND SITE PREPARATION OPERATIONS ALONG THE NEW ROAD ALIGNMENT.
- 1-2 COMMENCE WORK ACTIVITIES FOR THE NEW ROADWAY ALIGNMENT TO INCLUDE: GRADING, DRAINAGE, AGGREGATE, CURB AND GUTTER, SIDEWALK, PAVEMENT, MISC. CONCRETE, ETC. PAVEMENT SHALL BE PLACED UP TO THE INTERMEDIATE COURSE ONLY. WITH THE FINAL SURFACE COURSE TO BE PLACED IN ONE OPERATION AT THE END OF PHASE 2.
- 1-3 PERFORM FINAL GRADING OPERATIONS FOR THE WORK AREA SUBSTANTIALLY COMPLETED DURING THIS PHASE. SEEDING AND STABILIZATION SHALL BE COMPLETED

PHASE 2

TRAFFIC SHALL BE MAINTAINED IN GENERAL ACCORDANCE WITH TTC-5.2 AND TTC-16.2 DURING THIS PHASE OF CONSTRUCTION, AS PRESCRIBED IN THE GENERAL NOTES. TTC/MOT NOTES, AND THROUGH THE USE OF GROUP II CHANNELIZING DEVICES AND PROPER SIGNAGE. THIS PHASE OF WORK WILL COMPLETE THE CONNECTIONS AND FINAL INTERSECTION IMPROVEMENTS AT THE INTERSECTIONS WITH FAIRFAX BOULEVARD AND ORCHARD STREET.

- 2-1 INSTALL ALL SIGNING AND TRAFFIC CONTROL DEVICES PRIOR TO INTIATING WORK WITHIN THE EXISTING ROADWAY CORRIDORS. COMMENCE FINAL SITE PREPARATION FOR WORK AT THE ROADWAY INTERSECTIONS.
- 2-2 CONSTRUCT TIE-INS / PROPOSED INTERSECTIONS AT FAIRFAX BOULEVARD AND ORCHARD STREET. WORK CONSISTS OF SAW CUTS, PAVEMENT TIE-INS, SIDEWALK CONNECTIONS, AND FINAL GRADING. TRAFFIC SIGNAL MODIFICATIONS TO BE MADE AT THIS TIME WHILE MAINTAINING EXISTING OPERATIONS.
- 2-3 PERFORM FINAL SURFACE COURSE PAVING FOR THE ENTIRE PROJECT AND INSTALL PAVEMENT MARKINGS PER THE PROVIDED PLANS. FINAL IMPROVEMENTS TO INCLUDE LANDSCAPING, STORMWATER, AND LIGHTING SHALL FOLLOW.
- 2-4 UPON COMPLETION OF ALL WORK AND SITE CLEAN-UP, REMOVE TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES (AS APPROVED BY THE CITY/ENGINEER) AND ALL TEMPORARY TRAFFIC CONTROL DEVICES.

PLANS SUBJECT TO CHANGE

INFORMATION ONLY

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04/28/2020

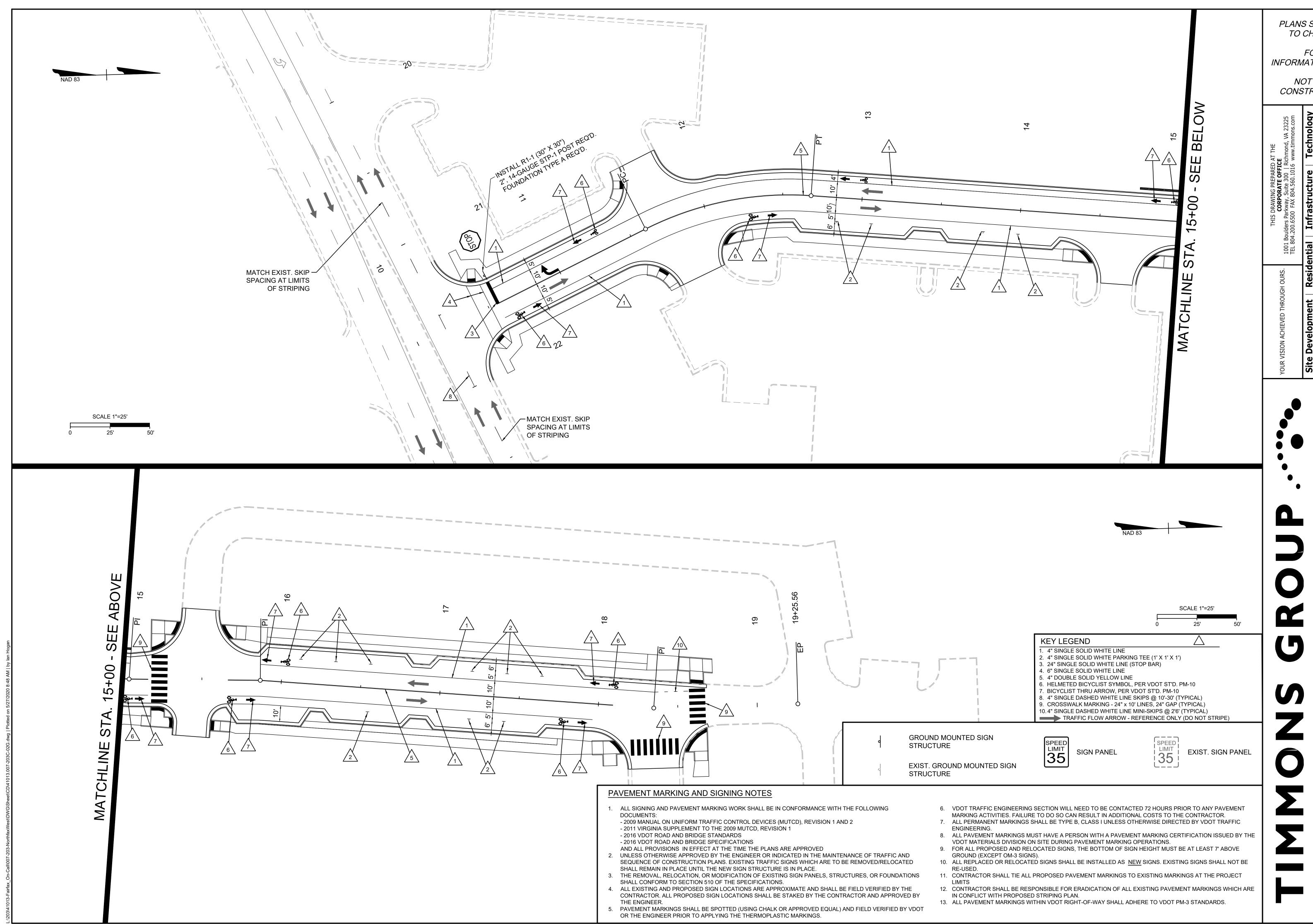
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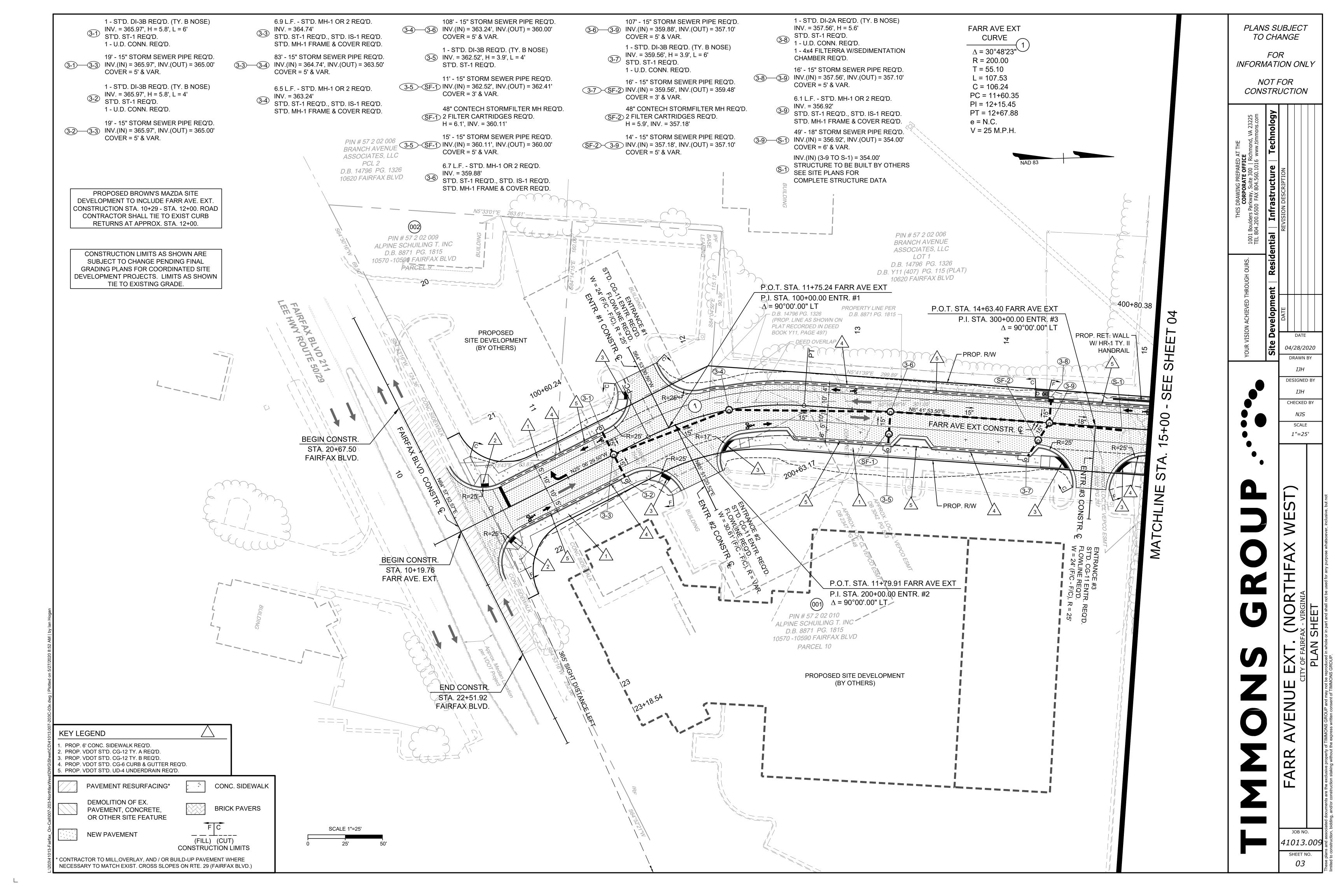
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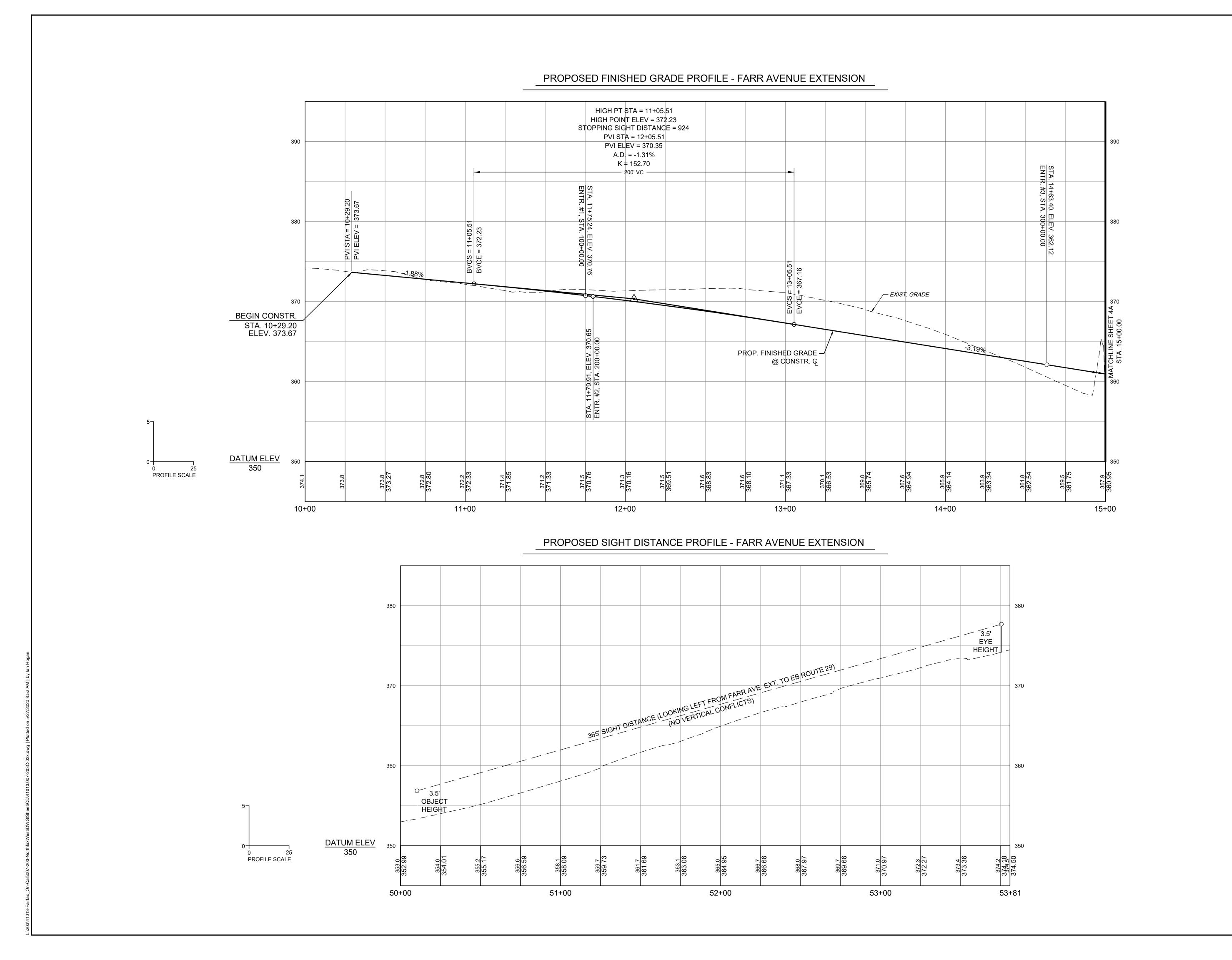
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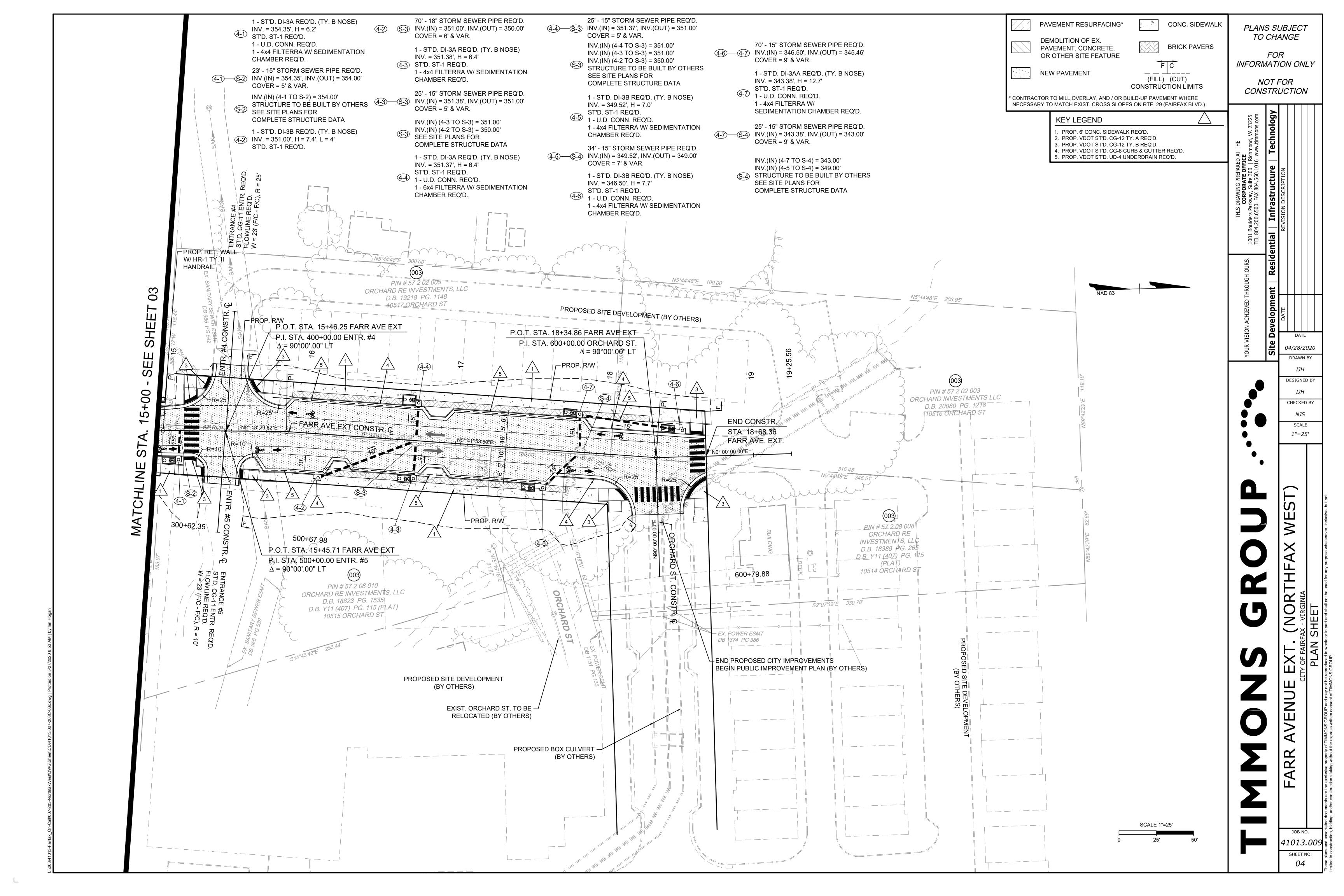
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JOB NO. SHEET NO.

41013.009 03A



PROPOSED FINISHED GRADE PROFILE - FARR AVENUE EXTENSION LOW POINT STA = 15+91.15 LOW POINT ELEV = 358.70 380 HIGH PT STA = 16+97.83 LOW POINT STA = 18+97.17 HIGH POINT ELEV = 357.63 LOW POINT ELEV = 353.66 STOPPING SIGHT DISTANCE = 608 PVI STA = 17+47.83 PVI STA = 18+62.17 PVI ELEV = 357.13 PV ELEV = 353.77 A.D. = -1.93% A.D. = 2.60% K = 26.97 K = 51.71 — 100' √C — — 70' VC — 370 PROP. FINISHED GRADE -PVI STA = 19+25.56 PVI ELEV = 353.56 @ CONSTR. Ç -1.00% -2.93% -0.34% EXIST. GRADE — PROFILE BEYOND 18+68.36 PROVIDED FOR INFORMATION ONLY 350 SEE SITE PLANS BY OTHERS END CONSTR. FOR CONTINUATION OF ROAD GRADING STA. 18+68.36 ELEV. 353.91 354.0 358.61 352.8 357.61 16+00 17+00 18+00 19+00 19+26

PVI STA = 15+61.15 PVI ELEV = 359.00

A.D. = 2.19%

K = 27.37

DATUM ELEV

15+00

PROFILE SCALE

---- 60' VC ------

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1"=25'

AVENUE

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41013.009 SHEET NO. 04A

SIDEWALK PROFILE - RTE. 29 TO ENTR. #1 (LEFT) SIDEWALK PROFILE - RTE. 29 TO ENTR. #2 (RIGHT) SIDEWALK PROFILES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY, TO PROVIDE CONTEXT TO SIDEWALK GRADING AND HIGHLIGHT AREAS WHERE SIDEWALK GRADE DEVIATES FROM FINISHED GRADE CENTERLINE PROFILE. SEE CROSS SECTIONS FOR ELEVATIONS OF – EXIST. GRADE FRONT AND BACK OF SIDEWALK. ←PROP. SIDEWALK GRADE CONTRACTOR SHALL TAKE NOTE THAT SIDEWALK PROFILES MAY VARY FROM CENTERLINE FINISHED PROP. SIDEWALK GRADE GRADE PROFILE THROUGHOUT CURBLINE TRANSITIONS OF BUMP-OUTS. CONTRACTOR SHALL ADJUST GRADE OF SIDEWALK AS REQUIRED TO MEET GRADES SHOWN ► EXIST. GRADE ON CROSS SECTIONS AND ACHIEVE TYPICAL SECTION. LONGITUDINAL SLOPE OF SIDEWALK SHALL REMAIN BETWEEN 0.40% AND 8% THROUGH THESE TRANSITIONS. ALL SIDEWALK PROFILES SHOWN ON THIS PLAN ARE BASED ON THE BACK (OUTSIDE) OF SIDEWALK. PROFILES SHOWN DO NOT INCLUDE CURB RAMPS OR DATUM ELEV DATUM ELEV AREAS WITHIN RADIAL RETURNS OF ENTRANCES OR INTERSECTIONS. 30+00 30+78 20+79 20+00 SIDEWALK PROFILE ENTR. #3 TO ENTR. #5 (RIGHT) SIDEWALK PROFILE - ENTR. #2 TO ENTR. #3 (RIGHT) 370 PROP. SIDEWALK GRADE EXIST. GRADE PROP. SIDEWALK GRADE CURBLINE CURBLINE
TRANSITION TRANSITION
ADJUSTMENT ADJUSTMENT CURBLINE CURBLINE TRANSITION EXIST. GRADE — ADJUSTMENT CURBLINE TRANSITION ADJUSTMENT **DATUM ELEV** DATUM ELEV PROFILE SCALE 40+00 41+00 41+93 42+67 42+81 SIDEWALK PROFILE - ENTR. #3 TO ORCHARD ST. (LEFT) SIDEWALK PROFILE - ENTR. #4 TO ORCHARD ST. (RIGHT) CURBLINE TRANSITION /ADJUSTMENT - PROP. SIDEWALK GRADE PROP. SIDEWALK GRADE CURBLINE TRANSITION -ADJUSTMENT CURBLINE EXIST. GRADE - TRANSITION EXIST. GRADE -ADJUSTMENT **DATUM ELEV** DATUM ELEV 350 60+00 61+00 62+00 62+58 50+00 51+00 52+00 52+26

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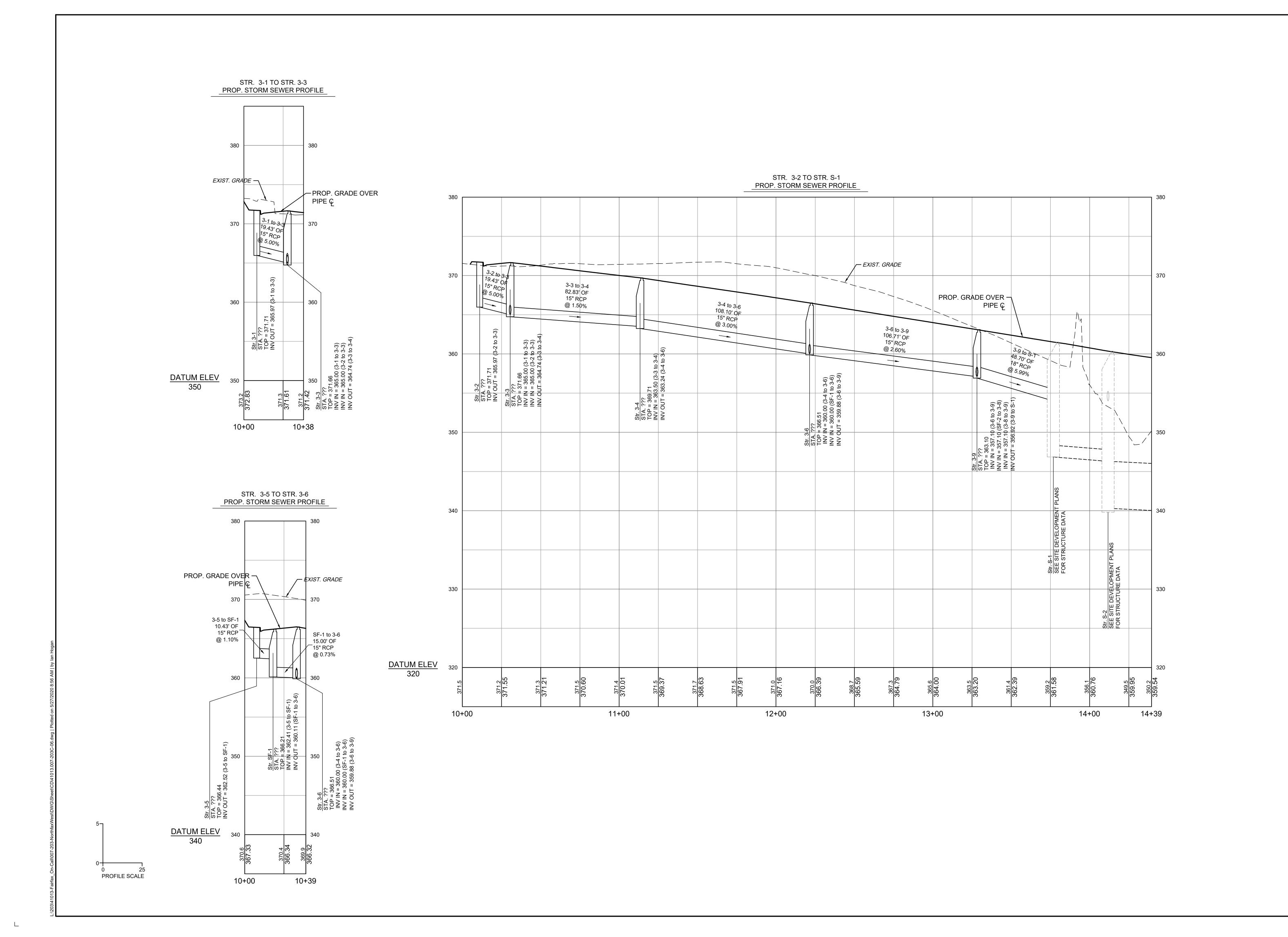
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Infrastructure | Technology

04/28/2020

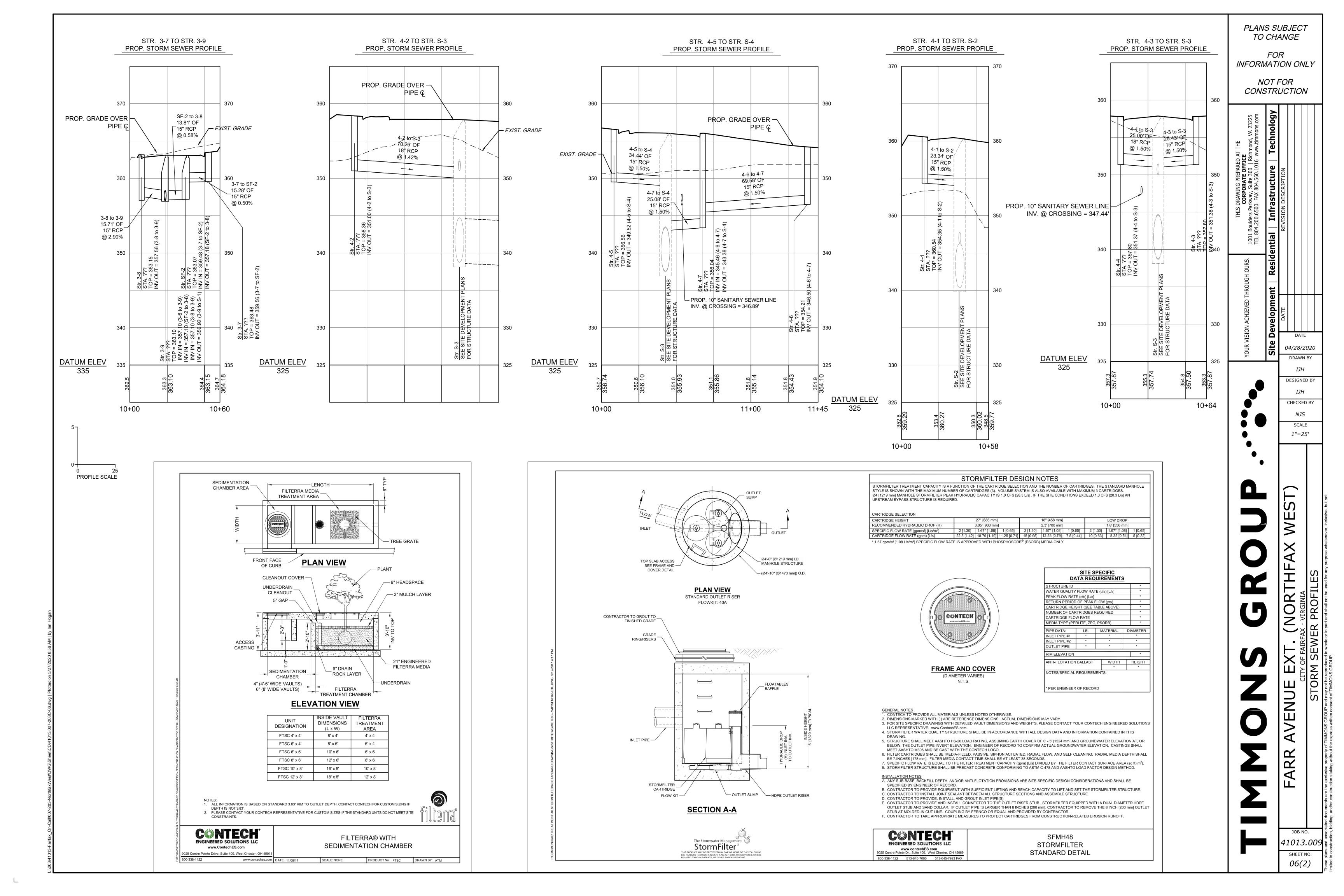
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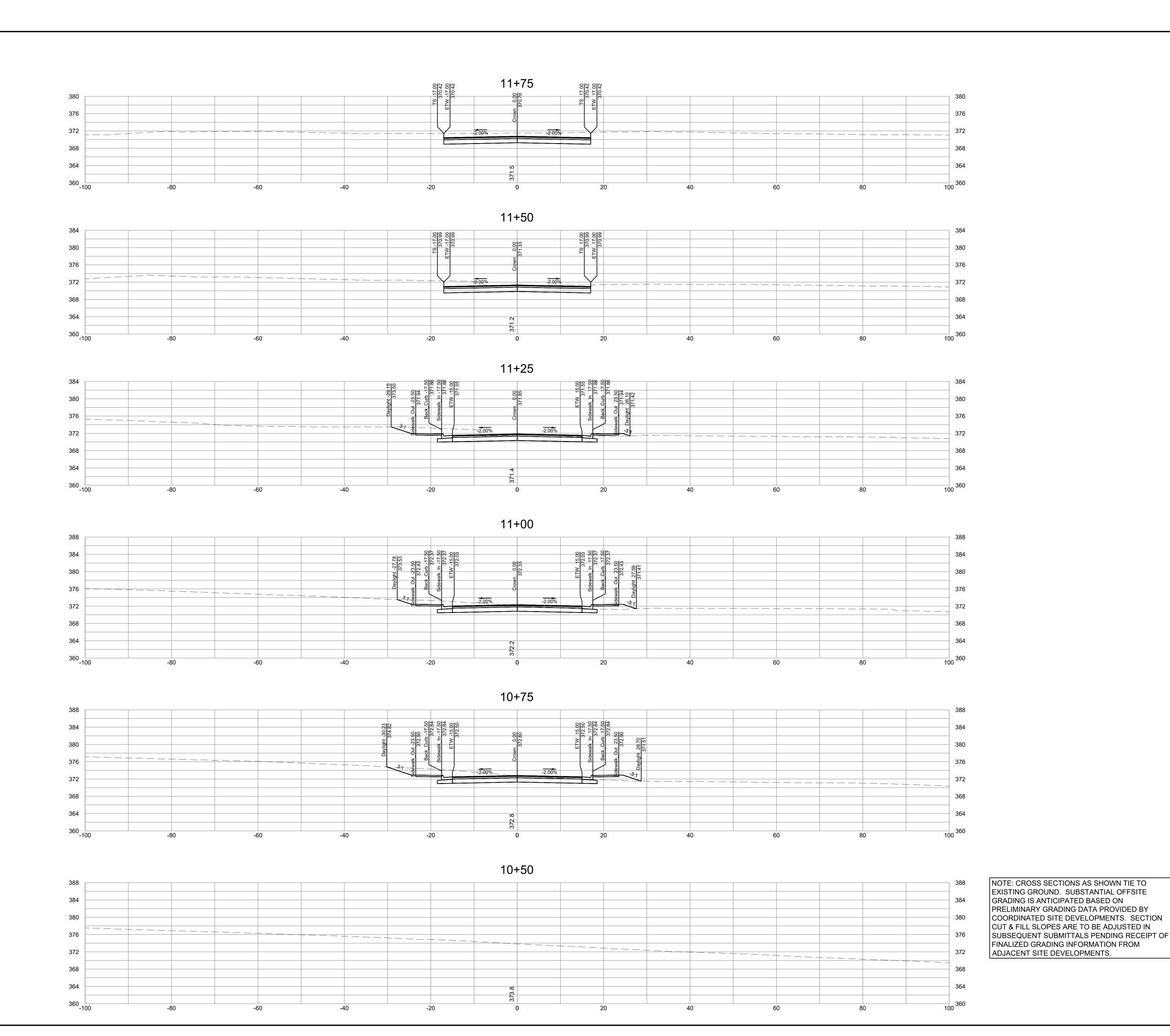
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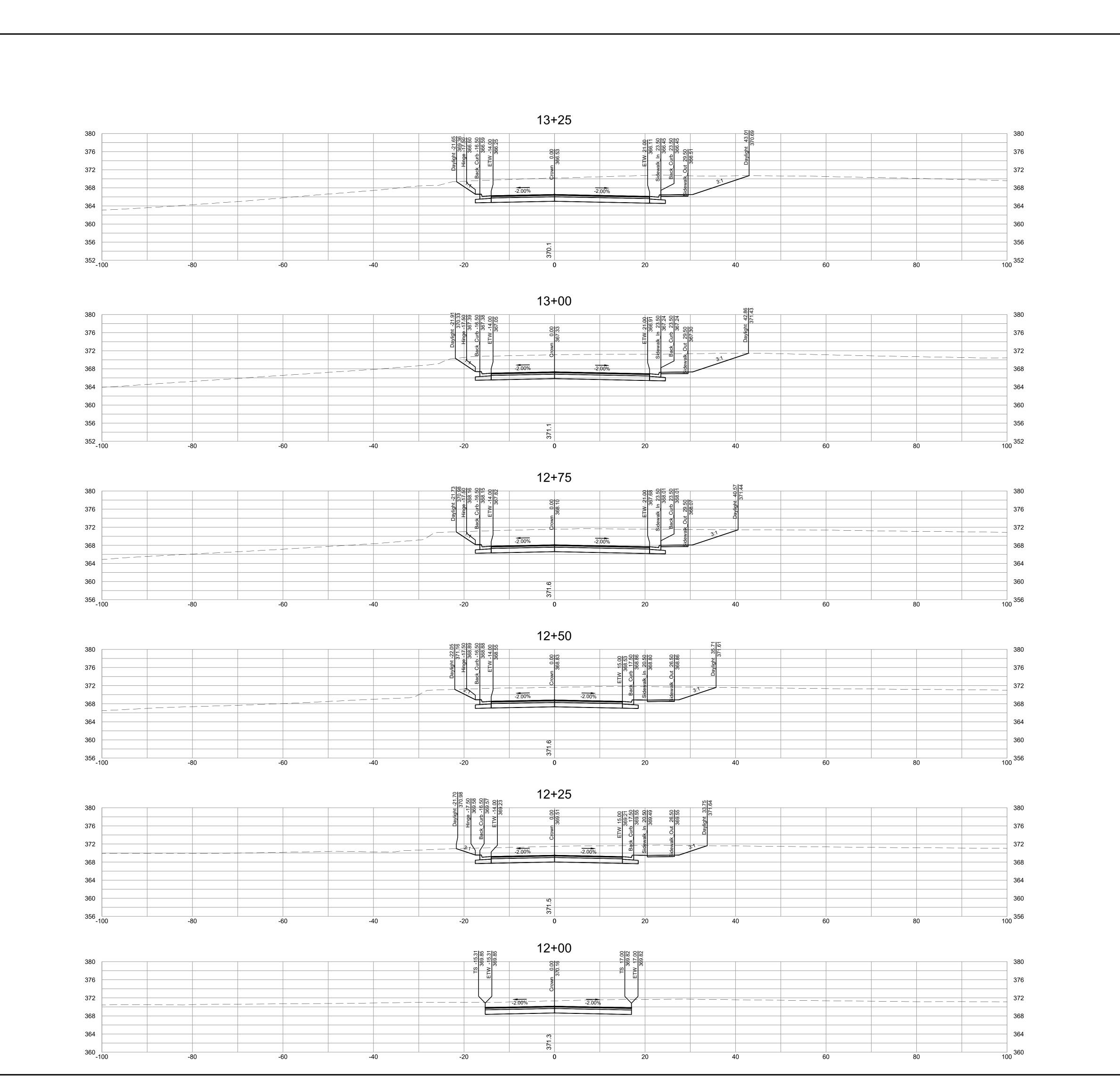


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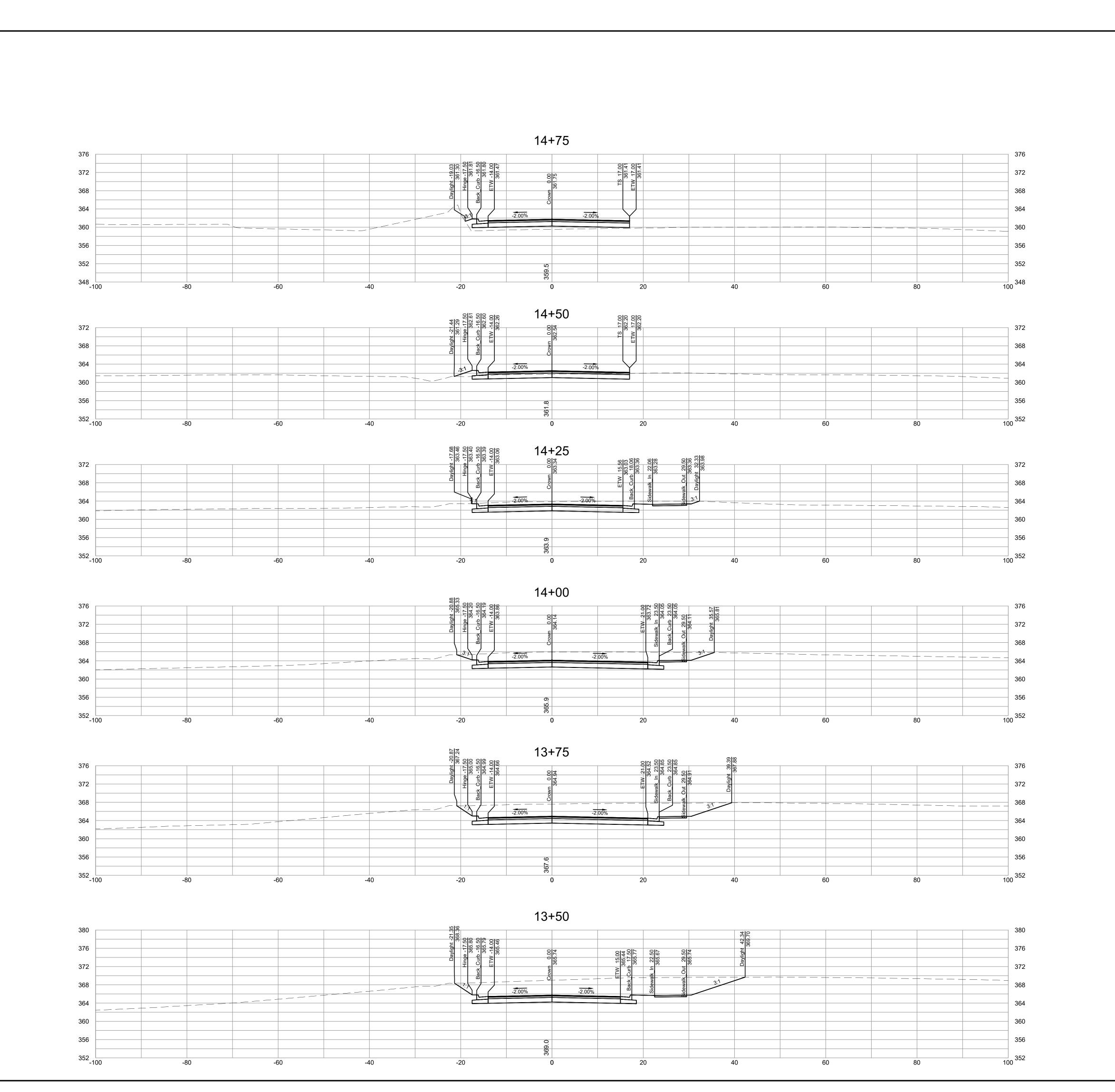
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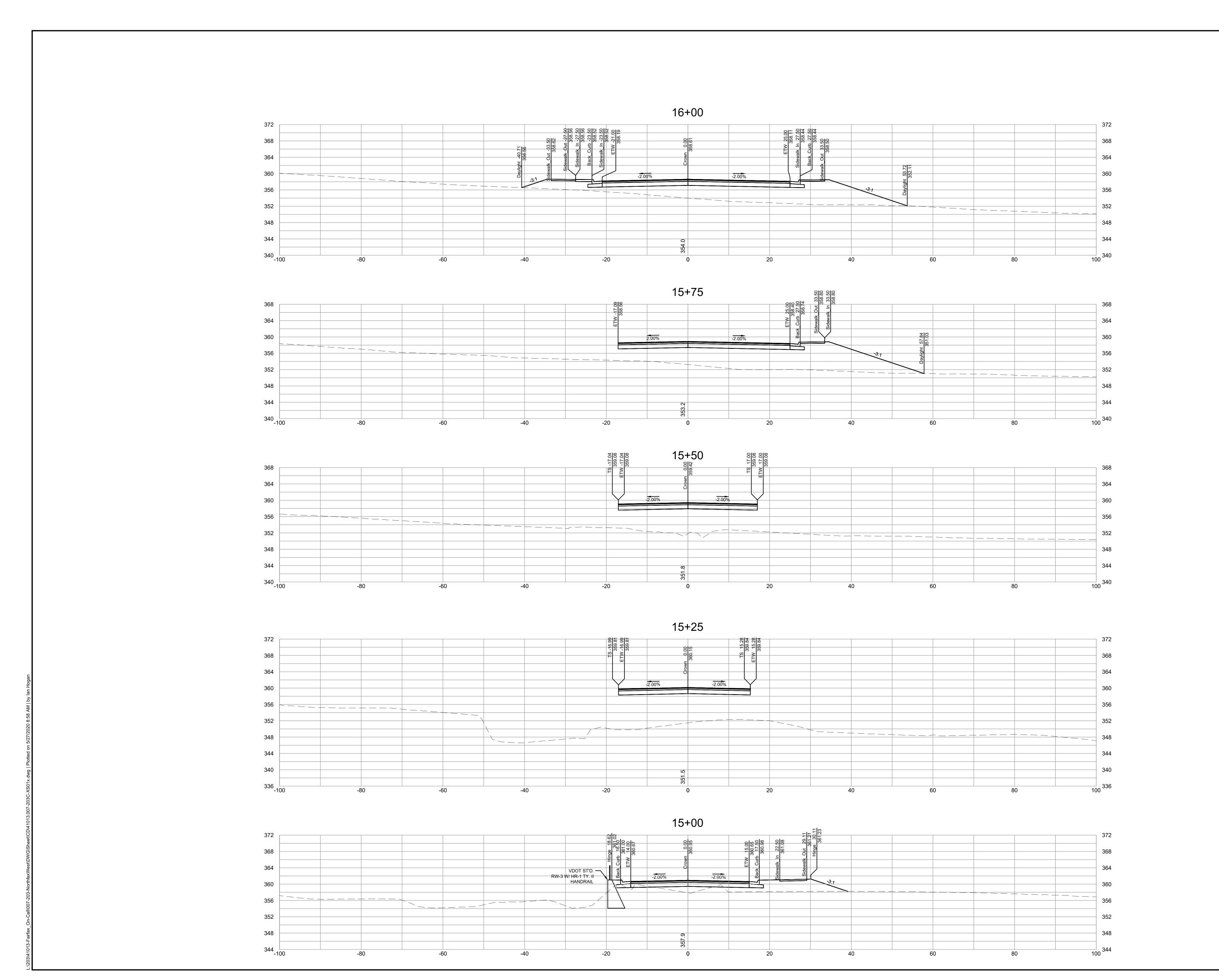
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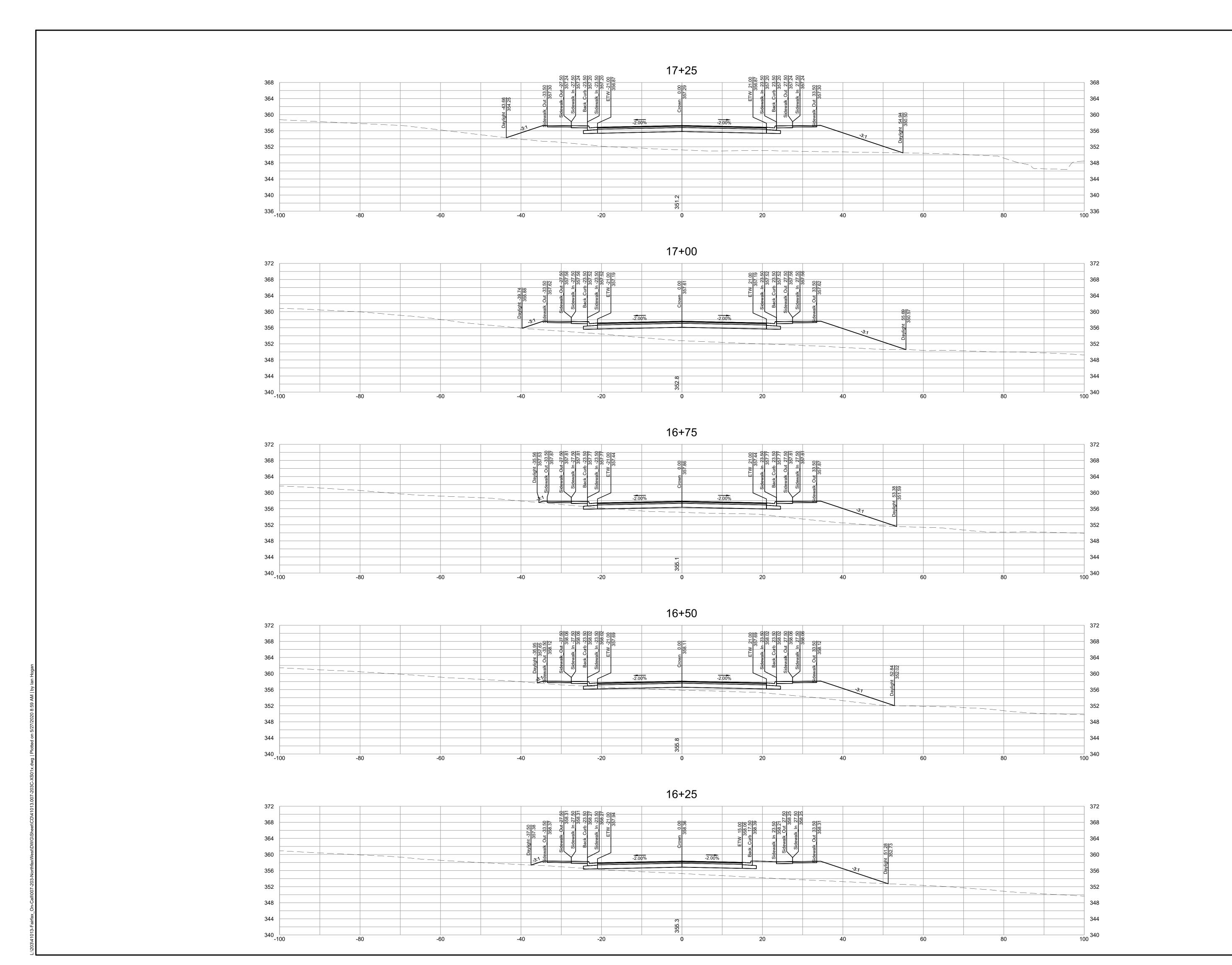
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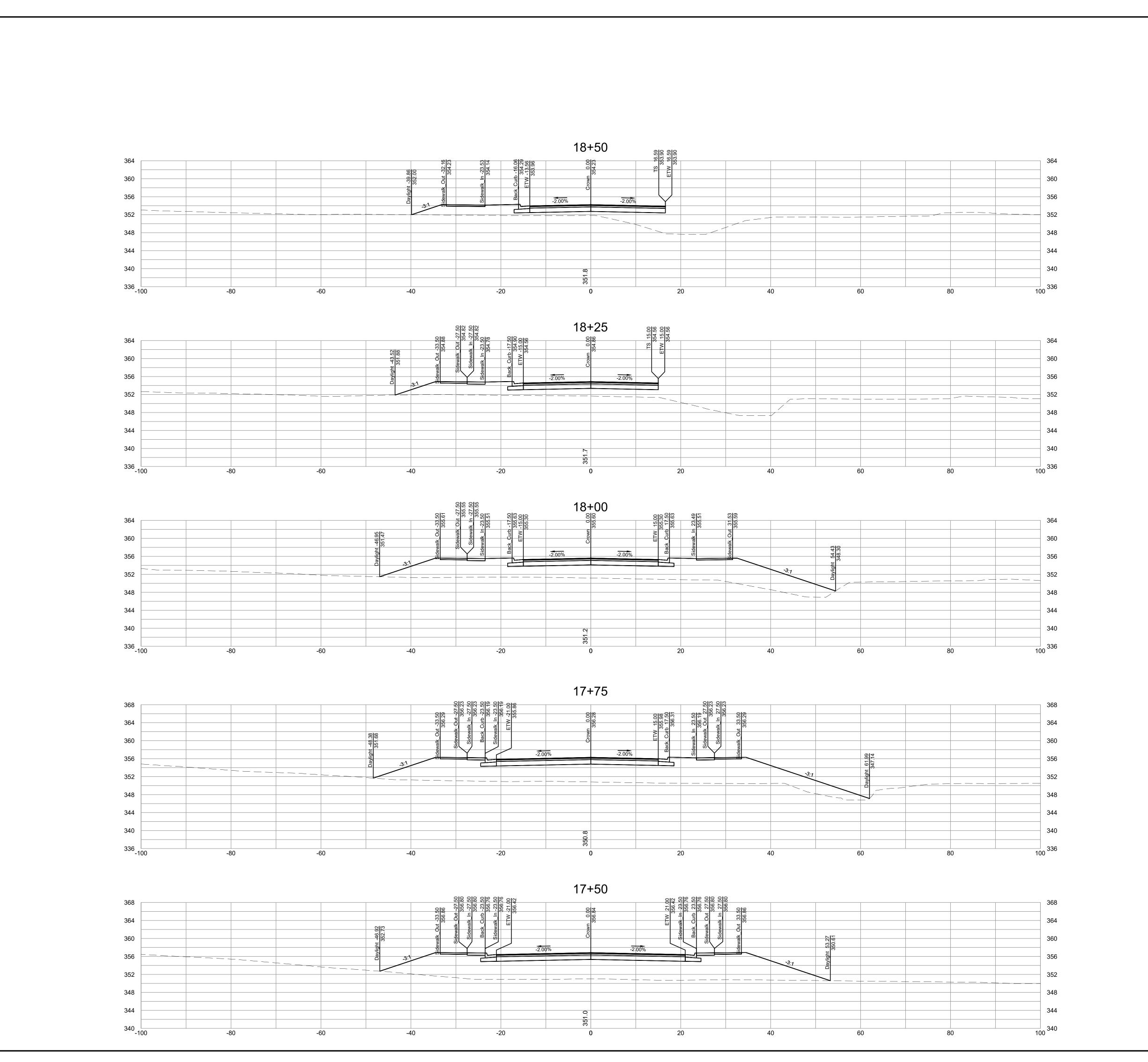
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FOR INFORMATION ONLY

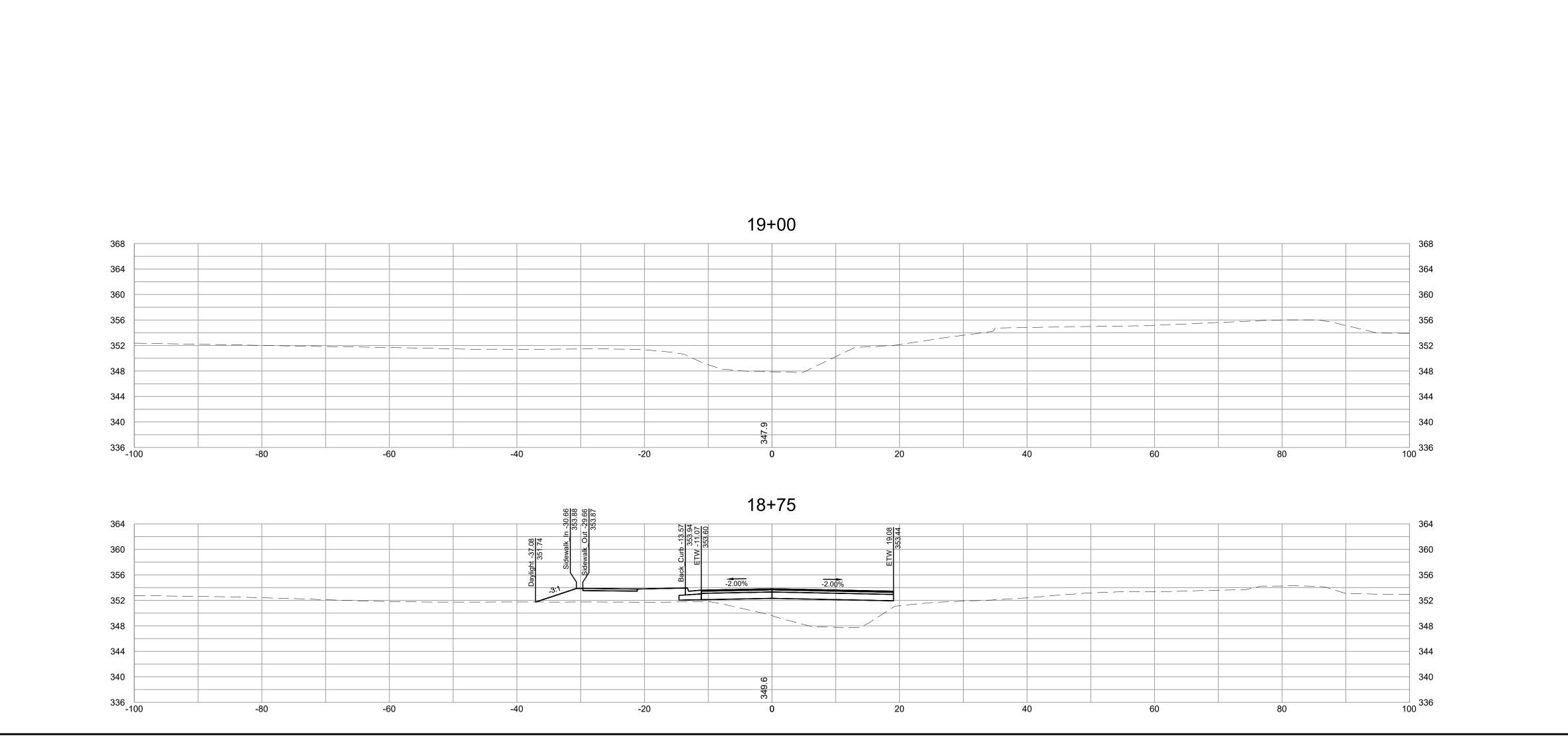
NOT FOR CONSTRUCTION

04/28/2020 IJH DESIGNED BY

NJS

CHECKED BY SCALE

1"=10'



FOR INFORMATION ONLY

NOT FOR

CONSTRUCTION

04/28/2020 IJH DESIGNED BY

SCALE 1"=10'